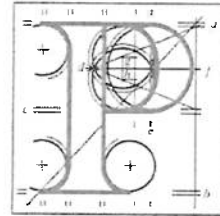


**Our Case Number: ABP-317660-23**



**An  
Bord  
Pleanála**

Chief Executive  
Dublin City Council  
Civic Offices  
Wood Quay  
Dublin 8

**Date:** 23 November 2023

**Re:** Busconnects Kimmage to city centre core bus corridor scheme  
Kimmage, Dublin

Dear Sir / Madam,

An Bord Pleanála has received your recent submission in relation to the above-mentioned proposed road development and will take it into consideration in its determination of the matter.

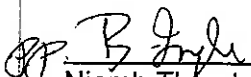
Please note that the proposed road development shall not be carried out unless the Board has approved it or approved it with modifications.

The Board has also received an application for confirmation of a compulsory purchase order which relates to this proposed road development. The Board has absolute discretion to hold an oral hearing in respect of any application before it, in accordance with section 218 of the Planning and Development Act 2000, as amended. Accordingly, the Board will inform you in due course on this matter. The Board shall also make a decision on both applications at the same time.

If you have any queries in relation to this matter please contact the undersigned officer of the Board at [laps@pleanala.ie](mailto:laps@pleanala.ie)

Please quote the above-mentioned An Bord Pleanála reference number in any correspondence or telephone contact with the Board.

Yours faithfully,

  
\_\_\_\_\_  
Niamh Thornton  
Executive Officer  
Direct Line: 01-8737247

HA02A

Teil	Tel	(01) 858 8100
Glaio Áitiúil	LoCall	1800 275 175
Facs	Fax	(01) 872 2684
Láithreán Gréasáin	Website	<a href="http://www.pleanala.ie">www.pleanala.ie</a>
Ríomhphost	Email	<a href="mailto:bord@pleanala.ie">bord@pleanala.ie</a>

64 Sráid Maoilbhríde	64 Marlborough Street
Baile Átha Cliath 1	Dublin 1
D01 V902	D01 V902

## Breda Ingle

---

**From:** LAPS  
**Sent:** Wednesday 22 November 2023 16:50  
**To:** 'fiona.brady@dublincity.ie'  
**Subject:** Bus Connects Dublin - Kimmage to City Centre BusConnects submission

Dear Fiona,

I am in receipt of your email, an official acknowledgement will issue in due course.

Kind regards,

Breda Ingle  
Strategic Infrastructure Development  
Ext. 7291

**From:** Fiona Brady <[fiona.brady@dublincity.ie](mailto:fiona.brady@dublincity.ie)>  
**Sent:** Wednesday, November 22, 2023 12:04 PM  
**To:** SIDS <[sids@pleanala.ie](mailto:sids@pleanala.ie)>  
**Cc:** Emer Uí Fhatharta <[emer.uifhatharta@dublincity.ie](mailto:emer.uifhatharta@dublincity.ie)>  
**Subject:** Bus Connects Dublin - Kimmage to City Centre BusConnects submission

To whom it may concern,

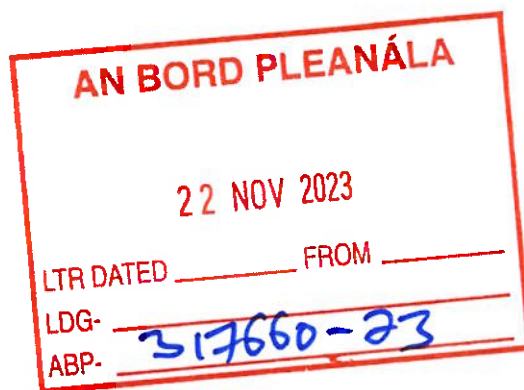
Please see attached Submission from Dublin City Council Chief Executive to An Bord Pleanála in relation to the National Transport Authority's BusConnects Dublin Kimmage to City Centre Core Bus Corridor Scheme.

Please acknowledge receipt of this submission at your earliest convenience.

Regards  
Fiona Brady  
On behalf of Emer Uí Fhátharta

**Smaoinigh ar an timpeallacht sula ndéanann tú an ríomhphost seo a phriontáil. Please consider the Environment before printing this mail.**

**Written Submission from Dublin City  
Council Chief Executive  
to An Bord Pleanála  
in relation to  
the National Transport Authority's  
BusConnects Dublin Kimmage to City Centre Core Bus Corridor Scheme  
An Bord Pleanála Ref: ABP-317660-23**



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Agreed conditions - Between Dublin City Council and the National Transport Authority

City Archaeologist

City Architects Department

Parks, Biodiversity and Landscape Services

Conservation Section

Environmental & Transportation Department

## **1.0 Kimmage to City Centre Core Bus Corridor Scheme**

The National Transport Authority has applied under Section 51 (2) of the Roads Act 1993 (as amended) to An Bord Pleanála for approval in relation to a proposed road development consisting of the Kimmage to City Centre Core Bus Corridor Scheme together with all ancillary and consequential works for the purpose of facilitating public transport.

### **1.1 Scope of Report**

In accordance with Section 51 (3)(b) of the Roads Act 1993 (as amended), this submission sets out the views of Dublin City Council (a prescribed body), on the Kimmage to City Centre Core Bus Corridor Scheme and the potential effects of the proposed development on the environment and the proper planning and sustainable development of the area.

In early 2019, as directed by the Chief Executive of Dublin City Council, a multi-disciplinary corporate team was established to provide a liaison role for the NTA BusConnects Project. The purpose of this team/office is to effectively manage the communications and act as the primary conduit for information exchange between Dublin City Council and the National Transportation Authority in relation to the BusConnects Programme.

This dedicated BusConnects Liaison Office has facilitated the exchange of information and engagement with other departments and sections within the City Council regarding the design of the bus corridors including the proposed scheme.

The BusConnects programme seeks to greatly improve bus services in Irish cities, including Dublin, so that journeys by bus will be fast, reliable, punctual, convenient and affordable. As set out in later section below, BusConnects is part of the Government's policy to improve public transport and address climate change in Dublin and other cities. BusConnects is included as a specific policy objective of Project Ireland 2040 – The National Development Plan 2018 – 2027 (Government of Ireland 2018); and the Climate Action Plan 2021 (Government of Ireland 2021).

## **2.0 Description of Proposed Development**

The proposed scheme is one of 12 stand-alone Core Bus Corridor (CBC) Schemes to be delivered under the BusConnects Dublin - Core Bus Corridors (CBC) Infrastructure Works. The CBC Infrastructure Works, once completed, will deliver the radial core bus corridors identified in the Transport Strategy for the Greater Dublin Area 2016 – 2035.

The proposed route is one of 12 arterial routes into the city centre, which are as follows:

- Clongriffin to City Centre Core Bus Corridor Scheme
- Swords to City Centre Core Bus Corridor Scheme
- Ballymun/Finglas to City Centre Core Bus Corridor Scheme
- Blanchardstown to City Centre Core Bus Corridor Scheme
- Lucan to City Centre Core Bus Corridor Scheme
- Liffey Valley to City Centre Core Bus Corridor Scheme
- Tallaght/Clondalkin to City Centre Core Bus Corridor Scheme
- Kimmage to City Centre Core Bus Corridor Scheme

- Tallaght/Clondalkin to City Centre Core Bus Corridor Scheme
- Bray to City Centre Core Bus Corridor Scheme
- Belfield/Blackrock to City Centre Core Bus Corridor Scheme.

The Kimmage to City Centre Core Bus Corridor Scheme comprises:

- 7.4 km (two-way) of bus priority infrastructure and traffic management;
- 8.0 k, (total both direction) of cycling infrastructure and facilities;
- Two new footbridges over the Grand Canal in Portobello;
- A new pedestrian/cyclist boardwalk structure over the river Poddle in Kimmage between Sundrive Road and Mount Argus Way;
- Provision of new/refurbished pedestrian facilities and footpaths along the scheme and associated ancillary works;
- Provision of 12 junction upgrades and associated ancillary works;
- Provision of 29 new/refurbished raised table side entry facilities;
- Reconfiguration of existing bus stops resulting in 23 number new bus stop facilities;
- Public Realm works including landscaping, planting, street furniture, street lighting, retaining walls, boundary walls and sustainable urban drainage (SUDs) measures;
- Roads associated earthworks including excavation of unacceptable material, importation of material, temporary storage of materials;
- Provision of road pavement, signing, lining and ancillary works;
- Provision of gates, fencing and boundary treatment works;
- Provision of new and diverted drainage infrastructure;
- Diversion of utilities and services including associated ancillary works;
- Construction of accommodation works including boundary treatment and ancillary grading and landscaping work together will all ancillary and consequential works associated therewith.

The Proposed Scheme has an overall length of approximately 3.7 km consisting of three sections, Lower Kimmage Road from Kimmage Cross Roads to the Junction with Harold's Cross Road; Harold's Cross Road from Harold's Cross Park to Grand Canal; and Clanbrassil Street Upper and Lower and New Street from the Grand Canal to the Patrick Street Junction.

The Lower Kimmage Road from Kimmage Cross Roads to the Junction with Harold's Cross Road section is approximately 2.2km long and commences on Kimmage Road Lower at the KCR Junction with Kimmage Road West, Fortfield Road and Terenure Road West running in a north-easterly direction. Priority for buses will be provided along the entire length of this section of the Proposed Scheme. A secondary cycle route is also designated, running parallel to Kimmage Road Lower, along Poddle Park, Bangor Road, and Blarney Park to Sundrive Road. From Sundrive Road, a new cycle connection to Mount Argus Way and Mount Argus View where a steel boardwalk structure is proposed beside the River Poddle at the Stone Boat feature.

From Harold's Cross Road and Harold's Cross Park the route proceeds towards the Grand Canal at Robert Emmet Bridge for a distance of 400 metres. Priority for buses will be provided along the entire length of this section of the Proposed Scheme, with retention and minor extension of the existing dedicated bus lanes along Harold's Cross Road. New segregated 1.5m wide cycle tracks are proposed in both directions along Harold's Cross Road.

At the Grand Canal the route proceeds from Robert Emmet Bridge over the Grand Canal on Clanbrassil Street Upper and through to the Leonard's Corner Junction at South Circular Road, and then along Clanbrassil Street Lower and New Street South, until it reaches the junction with Kevin Street Upper and Patrick Street. At Robert Emmet Bridge over the Grand Canal, two new cycle / pedestrian bridge structures are proposed on either side of the existing arch bridge to provide footpaths and the northbound cycle track outside of the narrow bridge width. Priority for buses will be provided. New segregated cycle tracks will be provided in both directions along the full length of this section of the Proposed Scheme.

The Construction Phase for the Proposed Scheme is anticipated to take approximately 18 months to complete. It will be constructed based on individual sectional completions that will individually have shorter durations typically ranging between three to 15 months.

Three Construction Compounds for the Proposed Scheme will be located at land adjacent to the Proposed Scheme at a number of locations. The Construction Compounds will be located at the following sites:

- Construction Compound K1 at Sundrive;
- Construction Compound K2 at Our Lady's Hospice; and
- Construction Compound K3 at St. Patrick's Court on Clanbrassil Street Lower.

Construction Compounds will be used as the primary location for the storage of materials, plant and equipment, site offices, worker welfare facilities and limited car parking. The Construction Compounds will be secured to ensure the safe storage of all on-site materials and machinery. Temporary fencing will be erected and site security will be employed.

A Construction Environmental Management Plan and a Construction Management Plan have been submitted with the application.

The NTA is a statutory non-commercial body, which operates under the aegis of the Department of Transport. The NTA was established on foot of the Dublin Transport Authority Act 2008 (as amended) (the '2008 Act'). In the case of the Proposed Scheme, the functions of the NTA include undertaking the design and planning process, seeking (and obtaining) all development consents including related compulsory acquisition approvals from An Bord Pleanála and constructing the Proposed Scheme (if approved).

## **2.1 Environmental Impact Assessment Report**

An EIAR has been submitted as part of the application and notes that the proposed scheme will address sustainable mode transport infrastructure deficits while contributing to an overall integrated sustainable transport system as proposed in the Greater Dublin Area Transport Strategy. It will increase the effectiveness and attractiveness of bus services operating along the corridor and will result in more people availing of public transport due to the faster journey times and reliability improvements which the Proposed Scheme provides. This in turn will support the potential to increase the bus network capacity of services operating along the corridor and thereby further increasing the attractiveness of public transport. In addition to this, the significant segregation and

safety improvements to walking and cycling infrastructure that is a key feature of the Proposed Scheme will further maximise the movement of people travelling sustainably along the corridor and will therefore cater for higher levels of future population and employment growth.

This report demonstrates how the proposed overall development accords with the Dublin City Development Plan 2022 – 2028 policies and objectives.

### **3.0 Context of Development**

#### **3.1 Relevant Planning History**

Significant planning applications adjacent to the route, within Dublin City, include:

- 4735/18**      **126-128 Harold's Cross Road, Dublin 6W**  
**Planning Permission granted** for the demolition of existing buildings and structures on site, with the exception of the front facade of no. 126 Harold's Cross Road; Construction of an infill residential development of 34 no. apartments with associated balconies/terraces comprising 18 no. 2 bedroom units, 11 no. 1 bedroom units and 5 no. studio units in 2 no. blocks (Block 1 & Block 2). Block 1 comprises a 5 storey (4 storey plus set-back penthouse level) over basement building to the west (rest) of the accommodating 31 no. units (17 no. 2 bedroom units, 10 no. 1 bedroom units and 4 no. studio units); Block 2 comprises a 2-3 storey over basement building to the east of the site (fronting onto Harold's Cross Road) accommodating 4 no. units (1 no. 2 bed unit, 1 no. 1 bed unit and 1 no. studio unit). Basement level to accommodate 30 no. car parking spaces, bicycle parking, refuse store and plant; Landscaped courtyard at ground floor podium level; Vehicular access from Harold's Cross Road, via 2 no. mechanised car lifts located in Block 2; All ancillary site development works, boundary treatment works and services.
- 3420/21**      **126-128 Harold's Cross Road, Dublin 6W**  
**Planning permission granted** for modifications to the previously approved permission DCC Reg Ref. 4735/18 - ABP 304552-19. Modifications are to include the addition of 6 no extra units achieved by way of an additional floor to the previously granted block 1, bringing the proposed height of the building from 5 storey to 6 storey and increasing the total units in the proposed development from 34 no. units (4 no. studio, 14 no. one bed, 13 no. two bed) up to 40 no. apartment units (4 no. studio, 17 one bed, 16 no. two bed). No modifications to block 2 are proposed from the scheme outlined in DCC Reg. Ref. 4735/18 - ABP-304552-19. No modifications to the no. of bicycle parking spaces (70 no.) and car parking spaces (30 no.) or basement layout are proposed. All associated signage, site works, drainage, street lighting and landscaping are as per the previously granted scheme.
- 3619/20**      **Site at 39, 40, 41, 42 & 42A, Clanbrassil Street Upper, Dublin 8**  
**Planning permission granted** for 1.) The demolition of existing buildings, structures and hardstanding areas on site except for the existing front part of the 2-storey take-away restaurant building (vacant) at 39 Clanbrassil Street Upper; 2.) The construction of a mixed-use development of 28 no. apartments with associated balconies/terraces comprising 17 no. 1 bedroom units & 11 no. 2 bedroom units and



2 no. commercial units located in 2 no. blocks (Block 1 & Block 2); 3.) Block 1 comprises a part2/part 2-storey plus pitched roof building at 39 Clanbrassil Street Upper accommodating 1 no. take-away restaurant unit at ground & 1st floor level (including existing structure fronting onto Clanbrassil Street Upper) and 4 no. apartments (4 no. 2 bedroom 2-storey own-door duplex apartment units) extending along Orr's Terrace; 4.) Block 2 comprises a 3-4 storey building at 40, 41, 42 & 42A Clanbrassil Street Upper accommodating 1 no. shop/retail services unit at ground floor level (fronting onto Clanbrassil Street Upper) and 24 no. apartments (17 no. 1-bedroom units and 7 no 2-bedroom units); 5.) Landscaped courtyard at ground floor level and roof terrace above third floor level in Block 2; 6.) Construction of new surface finishes and hard and soft landscaping to increased width Garden Terrace pedestrian laneway to the north; 7.) All ancillary site development works, plant areas, refuse storage areas, bicycle storage areas, boundary treatment works and services.

**4249/22**

**Site at 39 Clanbrassil Street Upper, Dublin 8**

**Planning permission granted** for change of use for the front part of the existing 2 storey take-away restaurant building (vacant). The application seeks modifications to Block 1 of the previously granted permission DCC Reg. Ref. 3619/20 (ABP-309667-21). Block 1 comprised of a part 2 storey/ part 2 storey plus pitched roof building at 39 Clanbrassil Street Upper accommodating 1 No. take-away restaurant unit at ground & 1st floor level (including existing structure fronting onto Clanbrassil Street Upper) and 4 No. apartments (4 No. 2 Bedroom 2 storey own-door duplex apartment units) extending along Orr's Terrace. The modifications seek change of use of previously granted take-away restaurant use to residential use to provide 1 No. Duplex Studio over ground and first floor. The proposed works include removal of existing signage and provision of a new entrance door to the front. All with associated bin store, bicycle store, and associated site works.

### **3.2 Policy Context**

#### **3.2.1 Regional Level**

##### **Regional Spatial and Economic Strategy for the Eastern and Midlands Regional Assembly (RSES) 2019-2031.**

The principal aim of the RSES is to support the implementation of Project Ireland 2040 by providing a long-term strategic planning and economic framework for the development of the Region. The RSES is underpinned by three key principles, i.e. placemaking, climate action and sustainable economic opportunity and growth. Sixteen Regional Strategic Outcomes (RSOs) are set out which are broadly aligned with the National Strategic Outcomes of the NPF. The RSES includes a more detailed Dublin Metropolitan Area Strategic Plan (MASP) which identifies strategic development and employment areas for population and employment growth, in addition to more generalised consolidation and re-intensification of infill, brownfield and underutilised lands within Dublin City and its suburbs.

The Dublin MASP sets out a list of key transport infrastructure investments in the Metropolitan Area as supported by National policy (RPO 8.7, RPO 8.9) to promote mobility management, sustainable transport use and the delivery of bus projects including Core Bus Corridors and Regional Bus

Corridors. The cycling objectives include delivery of the cycle network set out in the NTA's Greater Dublin Area Cycle Network Plan and investment priorities for cycleways. Overall, the RSES supports the delivery of key sustainable transport projects including BusConnects as set out in RPO 5.2.

### **3.2.2 Citywide Level – The Dublin City Development Plan 2022-2028**

#### **3.2.2.1 Sustainable Movement and Transport**

The City Development Plan is the statutory planning context for the assessment of development proposals. It sets out the policy context for the next six years to 2028. A significant number of policies have relevance for the delivery of transport infrastructure in the city.

The core strategy set out in the draft plan is to develop a low carbon, sustainable and climate resilient capital city, where people will choose to live, work, experience city living, invest and socialise. The vision for the city is that, within the next ten years, it will have an established international reputation as a city region that is one of Europe's most sustainable, dynamic and resourceful. It is envisaged that the city will be beautiful, compact city, with a distinct character and a vibrant culture, and with a diverse, green and innovation-based economy. The city will be a socially inclusive city of urban neighbourhoods based on the principle of the 15-minute city, which allows people's daily requirements to be reached within 15 minutes by foot, bicycle or public transport, and is therefore compact. All development will be connected by exemplary public transport, cycling and walking systems.

Dublin City Council (DCC) supports the improvement of public transport and cycling which will allow for higher density development, thereby creating a more sustainable interaction between land-use and transport. Chapter 8 of the Development Plan 'Sustainable Movement and Transport' sets out DCC policies and objectives which are relevant to Bus Connects. For convenience, relevant policies are quoted hereunder:

**SMT1 Modal Shift and Compact Growth** *To continue to promote modal shift from private car use towards increased use of more sustainable forms of transport such as active mobility and public transport, and to work with the National Transport Authority (NTA), Transport Infrastructure Ireland (TII) and other transport agencies in progressing an integrated set of transport objectives to achieve compact growth.*

**SMT3 Integrated Transport Network** *To support and promote the sustainability principles set out in National and Regional documents to ensure the creation of an integrated transport network that services the needs of communities and businesses of Dublin City and the region.*

**SMT4 Integration of Public Transport Services and Development** *To support and encourage intensification and mixed-use development along public transport corridors and to ensure the integration of high quality permeability links and public realm in tandem with the delivery of public transport services, to create attractive, liveable and high quality urban places.*

**SMT8 Public Realm Enhancements** *To support public realm enhancements that contribute to place making and liveability and which prioritise pedestrians in accordance with Dublin City Council's Public Realm Strategy ('Your City – Your Space'), the Public Realm Masterplan for the City Core (The Heart*

of the City), the Grafton Street Quarter Public Realm Plan and forthcoming public realm plans such as those for the Parnell Square Cultural Quarter Development and the City Markets Area.

**SMT11 Pedestrian Network** *To protect, improve and expand on the pedestrian network, linking key public buildings, shopping streets, public transport points and tourist and recreational attractions whilst ensuring accessibility for all, including people with mobility impairment and/or disabilities, older persons and people.*

**SMT12 Pedestrians and Public Realm** *To enhance the attractiveness and liveability of the City through the continued reallocation of space to pedestrians and public realm to provide a safe and comfortable street environment for pedestrians of all ages and abilities.*

**SMT14 City Centre Road Space** *To manage City Centre road-space to best address the needs of pedestrians and cyclists, public transport, shared modes and the private car, in particular, where there are intersections between DART, LUAS and Metrolink and with the existing and proposed bus network.*

**SMT19 Integration of Active Travel with Public Transport** *To work with the relevant transport providers, agencies and stakeholders to facilitate the integration of active travel (walking/cycling etc.) with public transport, ensuring ease of access for all.*

**SMT22 Key Sustainable Transport Projects** *To support the expeditious delivery of key sustainable transport projects so as to provide an integrated public transport network with efficient interchange between transport modes, serving the existing future needs of the city and region and to support the integration of existing public transport infrastructure with other transport modes. In particular the following projects subject to environmental requirements and appropriate planning consents being obtained:*

- *DART+*
- *Metrolink from Charlemont to Swords*
- *Bus Connects Core Bus Corridor projects*
- *Delivery of Luas to Finglas*
- *Progress and delivery of Luas to Poolbeg and Lucan*

### **3.2.2.2 Other Relevant Policies**

There are a significant number of City Development Plan policies with relevance to the delivery of transport in the city, including:

**SC1 Consolidation of the Inner City** *To consolidate and enhance the inner city, promote compact growth and maximise opportunities provided by existing and proposed public transport by linking the critical mass of existing and emerging communities such as Docklands, Heuston Quarter, Grangegorman, Stoneybatter, Smithfield, the Liberties and the North East Inner City and the south and north Georgian cores with each other, and to other regeneration areas.*

**SC8 Development of the Inner Suburbs** *To support the development of the inner suburbs and outer city in accordance with the strategic development areas and corridors set out under the Dublin Metropolitan Area Strategic Plan and fully maximise opportunities for intensification of infill, brownfield and underutilised land where it aligns with existing and pipeline public transport services and enhanced walking and cycling infrastructure.*

### **SC9 Key Urban Villages, Urban Villages and Neighbourhood Centres**

To develop and support the hierarchy of the suburban centres, including Key Urban Villages, Urban Villages and Neighbourhood Centres, in order to:

- support the sustainable consolidation of the city and align with the principles of the 15 minute city;
- provide for the essential economic and community support for local neighbourhoods; and
- promote and enhance the distinctive character and sense of place of these areas by ensuring an appropriate mix of retail and retail services.

**QHSN11 15-Minute City** *To promote the realisation of the 15-minute city which provides for liveable, sustainable urban neighbourhoods and villages throughout the city that deliver healthy placemaking, high quality housing and well designed, intergenerational and accessible, safe and inclusive public spaces served by local services, amenities, sports facilities and sustainable modes of public and accessible transport where feasible.*

**CEE12 Transition to a Low Carbon, Climate Resilient City Economy** *To support the transition to a low carbon, climate resilient city economy, as part of, and in tandem with, increased climate action mitigation and adaptation measures.*

## **3.2.3 Area Specific Plans and Policies**

### **3.2.3.1 Dublin City Development Plan 2022 – 2028: Strategic Development and Regeneration Areas (SDRAs)**

The Dublin City Settlement Strategy prioritises compact growth in the city centre, the network of urban villages outside the centre, and targeted growth along key transport corridors and at several sizeable opportunity sites, which are the Strategic Development and Regeneration Areas demarcated in the Plan. These are primarily brownfield lands located in both inner and outer city areas, where there is capacity to absorb a greater intensification of development owing to their proximity to public transport corridors and supporting urban infrastructure.

The SDRAs align with the Strategic Development Areas, as set out in the Regional Spatial and Economic Strategy for the Metropolitan Area Strategic Plan for Dublin. They are identified as areas capable of delivering significant quantum of homes and employment for the city, and each SDRA is accompanied by guiding principles set out under Chapter 13 of the Plan.

The proposed Core Bus Corridor passes within or alongside the Liberties and Newmarket Square SDRA 15 as identified in the City Development Plan. It is considered that the following objectives/principles of note for the proposed Core Bus Corridor.

### **3.2.3.1.1 Liberties and Newmarket Square SDRA 15**

The proposed scheme intersects SDRA 15 Liberties and Newmarket Square (which corresponds to the area defined in the Liberties Local Area Plan 2009), from R137 New Street South and its junction with Malpas Street to the junction with R110 Kevin Street, R137 Patrick Street and R110 Dean Street. The guiding principles for the SDRA relevant to the proposed scheme include:

- *'To maximise the potential benefit of the BusConnects Project to the Liberties area in terms of public realm improvements, green infrastructure and pedestrian and cycling infrastructure'.*
- *'To facilitate delivery of cycle routes identified in the NTA GDA Cycle Strategy'.*
- *'To encourage development that enhances the vitality of this emerging network of walking and cycling infrastructure'.*

### **3.2.3.1.2 Other Local Area Plans**

There is a commitment in the DCDP 2022-2028 to prepare a LAP for Harold's Cross.

### **3.2.3.1.3 Architectural Conservation Areas and Conservation Areas**

The proposed scheme overlaps the south-eastern edge of the Thomas Street ACA (2009) at Dean Street, however it is noted that no direct impacts are anticipated. DCC's policies relating to ACAs can be found in the Dublin City Development Plan 2022-2028. Policy BHA7 and BHA8 of the Dublin City Development Plan seek to protect the special interest and character of designated areas. Further areas in Dublin are being prioritised for inclusion as an ACA. Harold's Cross, through which the proposed scheme traverses, is one of these prioritised areas.

The Dublin City Development Plan 2022-2028 designates a number of areas as Conservation Areas. These are denoted by a red line conservation hatching on the zoning maps. The proposed scheme traverses the 'Grand Canal Conservation Area' and the 'Patrick Street Conservation Area'. Development within or affecting a Conservation Area must contribute positively to its character and distinctiveness and take opportunities to protect and enhance the character and appearance of the area and its setting, wherever possible. Policy BHA9 of the plan is of relevance to the proposed works. Refer to specific comment from DCC's Conservation section for detailed report and recommendations.

## **4.0 Planning Assessment**

### **4.1 Planning Policy**

In terms of Regional Policy, as set out in Section 2.2.1, the proposed scheme is supported by the RSES. BusConnects (of which the Proposed Scheme is a part) is identified as a key infrastructure project which will support the regional growth strategy for the Eastern and Midlands Region including the Dublin MASP area.

It is considered that the proposed scheme will contribute and support continued improved integration of transport with land use planning and the delivery of improved high-capacity Core Bus Corridors will enable and support the delivery of both residential and economic development opportunities, facilitating the sustainable growth of Dublin City and its metropolitan area. The RSES not only seeks an improved and enhanced bus network but also places cycling at the core of its transport objectives.

At citywide planning level, the Dublin City Development Plan 2022-2028 sets out policies and objectives required to achieve its Core Strategy. The proposal has been considered with regard to this Core Strategy and the policies and objectives of the current Dublin City Development Plan and in particular the dual aspirations of delivering necessary transport infrastructure to facilitate compact growth while also protecting Natura designated sites.

#### **4.2 Environmental Impact Assessment Report (EIAR)**

A comprehensive EIAR is provided with the application documents examining the project under all relevant impacts and finds generally that the development would not adversely impact on existing environmental amenities. As An Bord Pleanála is the competent authority with regard to the acceptability or otherwise of the EIAR, it is not the role of Dublin City Council to comment on the acceptability or not of the EIAR and its findings but the content points generally to the development having negligible impact on the existing environment.

#### **4.3 Natura 2000**

The Habitats Directive and the Birds Directive list habitats and species which are considered to be important and in need of protection. These sites are referred to as European Sites. Sites designated for wild birds are termed Special Protection Areas (SPAs) and sites designated for natural habitat types or other species are termed Special Areas of Conservation (SACs). The network of European sites is referred to as Natura 2000.

An Appropriate Assessment Screening report has been prepared by the applicant which concludes that, having regard to the nature of the project and its potential relationship with all European sites within the zone of influence, and their conservation objectives, it is the professional opinion of the authors of this report that the application for approval for the proposed scheme does require a Stage Two Appropriate Assessment in respect of the 16 European sites (5 SACs and 12 SPAs) and consequently the preparation of a Natura Impact Statement (NIS).

The Natura Impact Statement prepared by the applicant identifies the following SPA and SAC designated areas in the vicinity of the proposed development, with the approximate distance from the site which were considered to be within the scheme's potential zone of influence.

<b>European Site</b>	<b>Distance from Scheme (KM)</b>
<u>Special Areas of Conservation (SACs)</u>	
Rye Water Valley / Carton SAC [001398]	13.8
South Dublin Bay SAC [000210]	3.9
North Dublin Bay SAC [000206]	6.5
Rockabill to Dalkey Island SAC [003000]	12.1
Howth Head SAC [000202]	12.2
Wicklow Mountains SAC [002122]	8.1
Knocksink Wood SAC [000725]	12.4
Ballyman Glen SAC [000713]	14.3
Baldoyle Bay SAC [000199]	11.5
Glenasmole Valley SAC [001209]	7.4

Ireland's Eye SAC [002193]	15.4
Malahide Estuary SAC [000205]	14.3
Rogerstown Estuary SAC [000208]	18.5
Lambay Island SAC [000204]	22.9
<u>Special Protection Areas (SPAs)</u>	
South Dublin Bay and River Tolka Estuary SPA [004024]	3.6
North Bull Island SPA [004006]	6.5
Dalkey Islands SPA [004172]	13
Wicklow Mountains SPA [004040]	8.2
Baldoyle Bay SPA [004016]	11.7
Howth Head Coast SPA [004113]	14.7
Ireland's Eye SPA [004117]	15.2
Malahide Estuary SPA [004025]	14.3
Rogerstown Estuary SPA [004015]	18.8
Lambay Island SPA [004069]	22.7
The Murrough SPA [004186]	28.4
Skerries Islands SPA [004122]	28.3
Rockabill SPA [004114]	28.5

The NIS notes that the proposed scheme does not overlap with any European sites, although it is hydrologically connected to Dublin Bay via the receiving surface water network. A table of potential impacts is set out and mitigation measures are identified.

Dublin City Council considers that the submitted Natura Impact Statement is generally satisfactory in terms of identifying the relevant Natura 2000 sites and the potential adverse impacts on the integrity of designated Natura 2000 sites along the Dublin coastline in view of their conservation objectives. There is considered to be sufficient distance from the intended route of the bus corridor to SAC and SPA sites, and the avoidance, design requirements and mitigation measures set out in the NIS will ensure that any impacts on the conservation objectives of European sites will be avoided during the construction and operation of the proposed scheme such there will be no adverse effects on any European sites.

The Natura Impact Statement objectively concludes that the development will not adversely affect (either directly or indirectly) the integrity of any European site, either alone or in combination with other plans or projects and that there is no reasonable scientific doubt in relation to this conclusion.

#### **4.4 Zoning and other designations**

##### **4.4.1 Land-Use Zoning Objectives**

The area along the proposed route includes lands which are subject to the following land-use zoning objectives under the Dublin City Development Plan 2022 - 2028:

- Z1 (Sustainable Residential Neighbourhoods)
- Z2 Residential Neighbourhoods (Conservation Areas)

- Z3 (Neighbourhood Centres)
- Z4 (Key Urban Villages / Urban Villages)
- Z6 (Employment/Enterprise)
- Z8 (Georgian Conservation Area)
- Z9 (open space)
- Z10 (Inner Suburban and Inner City Sustainable Mixed Uses)
- Z14 (Strategic Development and Regeneration Areas (SDRAs))
- Z15 (Community and Social Infrastructure).

The proposed scheme will, for the most part, will comprise lands within the existing public road and pedestrian pavement area where there is no specific zoning objective. The areas required for Construction Compounds will be for a temporary period. Reinstatement works will be carried out following construction.

The Dublin City Development Plan 2022-2028 defines a 'public service installation' as *'a building, or part thereof, a roadway or land used for the provision of public services. Public services include all service installations necessary for electricity, gas, telephone, radio, telecommunications, television, data transmission, drainage, including wastewater treatment plants and other statutory undertakers: bring centres, green waste composting centres, public libraries, public lavatories, public telephone boxes, bus shelters, etc. but does not include incinerators/waste to energy plants. The offices of such undertakers and companies involved in service installations are not included in this definition.'*

As defined above, the secondary elements associated with the proposed scheme, such as bus shelters, stops and real time information signage fall within the definition of public service installation.

Public service installations are permitted in principle under zoning objectives Z1, Z2, Z3, Z4, Z5, Z6, Z9, Z10, Z14 and Z15. They are open for consideration under zoning objective Z8.

Overall, is considered that the proposals would be compatible and consistent with the zoning objectives for the area.

#### **4.5 Impact on amenity**

Dublin City Council is satisfied that the elements of the proposed development which fall within the Council boundary would not have any excessive or undue impact on the amenities of the area. There will be a degree of disruption in terms of traffic management during construction but thereafter there is unlikely to be adverse impact on existing amenities. There will be a need for sharing of space including kerbside space, which will need to be managed to ensure that there is no undue adverse impact on the ability of residents and visitors to access local services on foot or on the ability to achieve the '15-minute city'.

#### **4.6 Forward Planning Comment**

In general, the proposed scheme is supported by the high level policies in place in the current Dublin City Development Plan 2022-2028. It is requested that those citywide and area specific policies and



objectives mentioned above be taken into account when the proposed works along the Kimmage to City Centre Core route are being formally considered.

#### **4.7 Departmental Reports**

The following Dublin City Council Departments and Divisions submitted a report and their response has been incorporated into the Planning Authority's Report:

- City Archaeologist
- City Architects Division
- Parks, Biodiversity & Landscape Services
- Conservation Section
- Environment and Transportation Department – including comments from Traffic, Roads, Public Lighting and Environmental Protection Divisions

Additional comments from the various departments etc. are provided in the appendix.

#### **4.8 City Archaeologist**

The corridor for the proposed Kimmage to City Centre scheme runs from the Kimmage Cross Roads to the junction of New Street South and Kevin Street Upper. The scheme runs adjacent to the River Poddle for much of its length and passes through a number of Zones of Archaeological Potential for Recorded Monuments which are listed on the Record of Monuments and Places (RMP) and are subject to statutory protection under Section 12 of the National Monuments (Amendment) Act 1994. The scheme will also impact sites listed on the Dublin City Industrial Heritage Record. Archaeological mitigation in these areas will be required where subsurface excavation is proposed.

The proposed construction of a boardwalk along the River Poddle will have a direct and permanent impact on the setting of Recorded Monument DU018-043003---, known as the 'Tongue' or 'Stoneboat' which is discussed further below.

#### **EIAR**

The archaeological and cultural heritage impacts of the construction phase and operational phase associated with the construction and operation of the Kimmage to City Centre Core Bus Corridor Scheme are assessed in Chapter 15 of the Environmental Impact Assessment Report (EIAR). This report provides a desk study of published and unpublished documentary and cartographic sources, supported by a field survey. The findings of the report are summarised below.

The EIAR lists several sites/areas of historical and cultural heritage importance along the route. Starting at the Kimmage Cross Roads the scheme is within the Zone of Archaeological Potential for the medieval City Watercourse/River Poddle (DU022-003) and close to the DCIHR site of Ravensdale Mills (DCIHR22-02-011). Further north a boardwalk is proposed along a short stretch of the Poddle at the site of the 'Tongue' (DU018-043003), a medieval water engineering feature. The scheme runs past Harold's Cross Park on both sides. Harold's Cross is believed to derive its name from a medieval cross which marked the boundary between the medieval manor of St. Sepulchre, owned by the Archbishop of Dublin, and the lands of the powerful Harold family. The site of the park is a Recorded Monument (DU018-050) and a gallows (DU018-050001) and a maypole (DU018-0004) are recorded as standing within the site. The scheme crosses the Grand Canal at Robert Emmett Bridge, which is

listed on both the DCIHR (DCIHR18-15-009) and on the National Inventory of Architectural Heritage (NIAH 50080983). Footbridges are proposed flanking the current bridge at this location, as well as the removal of sections of historic walling. The scheme then runs along Clanbrassil Street, past the (Fictional) birthplace of Leopold Bloom, and enters the Zone of Archaeological Potential for the Historic City of Dublin (DU018-020), passing over the site of a former defensive gateway (DU018-020200). This is identified in the EIAR as a potential National Monument following the guidance of the National Policy on Town Defences (2008). The scheme ends at the junction of New Street South and Kevin Street Upper, the crossroads known historically as the 'Four Corners of Hell'. Thus, unlike the choice faced by Brendan Behan of going "*to hell or to Kimmage*", the bus corridor will allow swift journeys to both Hell *and* to Kimmage.

In total the EIAR identifies impacts on one site designated as a National Monument, eight sites listed on the Records of Monuments and Places (RMP), and five sites listed on the Dublin City Industrial Heritage Record (DCIHR). There is also a potential impact on one non designated cultural heritage site.

Section 15.4.1 of the EIAR provides the main potential impacts on archaeology and cultural heritage as a result of construction works could arise from:

- Pavement construction, repairs, and reconstruction works;
- Road resurfacing works;
- Any excavations of soil, including landscaping works and ground disturbance for utility works; and
- Any ground disturbance for utility works.

The EIAR proposes that all subsurface archaeological and cultural heritage issues be resolved by archaeological mitigation during the pre-construction phase and/or construction phase, in advance of the operational phase, through one or more of the following mitigations:

- Preservation by record (archaeological excavation);
- Preservation in situ;
- Preservation by design; and
- Archaeological monitoring.

Section 15.5 of the EIAR addresses the proposed archaeological mitigation measures as follows:

- An experienced and competent licence-eligible archaeologist will be employed by the appointed contractor to advise on archaeological and cultural heritage matters during construction, to communicate all findings in a timely manner to the NTA and statutory authorities, to acquire any licenses/ consents required to conduct the work, and to supervise and direct the archaeological measures associated with the Proposed Scheme.
- Licence applications are made by the licence-eligible archaeologist to the National Monuments Service at the DHLGH. In addition to a detailed method statement, the applications must include a letter from the NTA that confirms the availability of adequate funding. There is a prescribed format for the letter that must be followed.
- Other consents may include a Detection Device licence to use a metal-detector or to carry out a non-invasive geophysical survey.

- A construction schedule will be made available to the archaeologist, with information on where and when the various elements and ground disturbance will take place.
- As part of the licensing requirements, it is essential for the client to provide sufficient notice to the archaeologist(s) in advance of the construction works commencing. This will allow for prompt arrival on site to undertake additional surveys and to monitor ground disturbances. As often happens, there may be down time where no excavation work is taking place during the construction phase. In this case, it will be necessary to inform the archaeologist/s as to when ground-breaking works will recommence.
- In the event of archaeological features or material being uncovered during the Construction Phase, all machine work will cease in the immediate area to allow the archaeologist/s time to inspect and record any such material.
- Once the presence of archaeologically significant material is established, full archaeological recording of such material is recommended. If it is not possible for the construction works to avoid the material, full excavation will be recommended. The extent and duration of excavation will be advised by the client's archaeologist and will be a matter for discussion between the NTA and the licensing authorities.
- Secure storage for artefacts recovered during the course of the monitoring and related work will be provided by the appointed contractor.
- As part of the licensing requirement and in accordance with the funding letter, adequate funds to cover excavation, post-excavation analysis, and any testing or conservation work required will be made available.
- During the construction all machine traffic must be restricted as to avoid any newly revealed archaeological or cultural heritage sites and their environs. Materials management will be in operation to ensure no damage to a site of archaeological interest

Archaeological monitoring (as defined in section 15.5.1) under licence will take place, where any preparatory ground-breaking or ground reduction works are required (as defined in section 15.4.1), at all sites of archaeological and cultural heritage along the proposed route, including National Monuments, Recorded Monuments and sites listed in the DCIHR.

It is in these areas that there is a possibility to disturb intact archaeological layers and material. Licensed archaeological excavation, in full or in part, of any identified archaeological remains (preservation by record) or preservation in situ will be undertaken.

#### The 'Tongue'

In the medieval period the River Poddle was the chief water supply to Dublin City and provided not just fresh drinking water but also motive power to drive mill wheels. It was vital therefore not just to the health of the city, but also to its economy. This importance led to frequent conflict between the civic authorities and the Abbey of St. Thomas the Martyr which was politically independent of the city and through whose lands the river flowed. Agreement was reached between the City and the Abbey that the waters of the Poddle would be shared, with two thirds of the flow being allocated to the Abbey and the remaining one third allocated to the City. To effect this division a water control feature known as 'The Tongue' (DU018-043003) was constructed along with a new water channel. This split the river, diverting part to the Old City Water Course, and allowing the rest to flow on to the Abbey. Along with other features such as the Balrothery Weir, which was also constructed in the

medieval period to divert extra water from the Dodder into the Poddle, the Tongue is a key part of the city's medieval and engineering heritage and shows the level of effort and skill exerted in the past to ensure the supply of water.



Figure 1. The current appearance of the 'Tongue'. An inscription on the north face states "STONE BOAT/BUILT 1245 A.D./RESTORED BY/TIERNAN BUILDERS/1990 A.D."



Figure 2. The Tongue in 1977, pre reconstruction. It is unknown if historic fabric survives in the core of the current structure.

Although subjected to what would now be considered to be a highly inappropriate "restoration" in 1990 which may have destroyed historic fabric, the monument remains important locally and still performs its original function of water division. Nicknamed the 'Stoneboat' due to its shape, it lends its name to a nearby public house on Sundrive Road. Housing development in the surrounding area has removed its original context however and without any interpretation the monument is easily overlooked. Proposals to enhance and interpret the feature as part of this scheme are therefore



welcome. The current plans are however lacking in detail and it is not clear that they will deliver the stated *“Positive, Moderate, Long Term residual impact”* stated without revision.



Figure 3. Photomontage of the boardwalk. Note the lack of visibility through the deck.

The currently designed boardwalk runs over the top of the Tongue. It is stated that *“The boardwalk deck will be perforated such that the Stone Boat will be visible through it”* however it is not clear from the supplied drawings and photomontage images that this will be the case. No photomontage is presented showing the view of the Tongue from the boardwalk. As presented, there is a uniform *“perforated metal deck with slip resistance”*. No attempt is made in the deck design to distinguish the area above the monument and the water courses. It is likely, therefore, that the effect will be to obscure the monument. It is submitted that consideration should be given to a redesign of the deck to highlight the Tongue, perhaps including glass or similar transparent panels directly above it to ensure visibility.

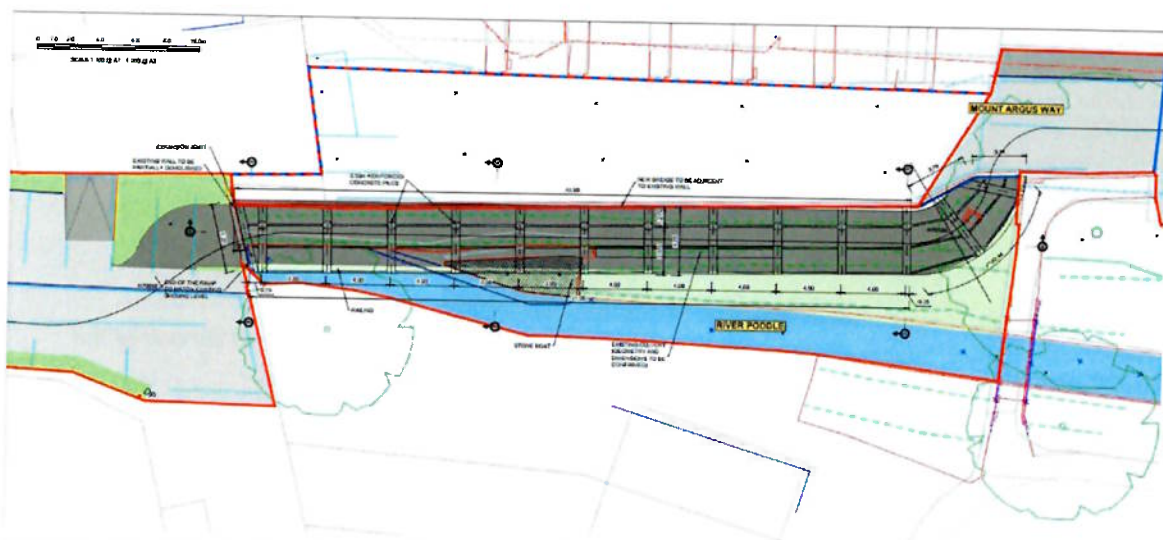


Figure 4. Plan of the proposed boardwalk. Note lack of features highlighting the watercourse or monument beneath.

It is also stated that information panels will be installed. Two are shown on the provided drawings. No details of design or content are provided however. These should be carefully designed and written, providing information not just on the immediate monument, but also allowing the viewer to situate it in its wider context of the River Poddle and the city's water supply. Local groups have recently been in touch with the DCC Heritage Officer separately from the current Bus Connects proposal seeking to get interpretive signage installed at this location. Ideally, the information panels should be designed in conjunction with DCC and with input from local heritage groups.

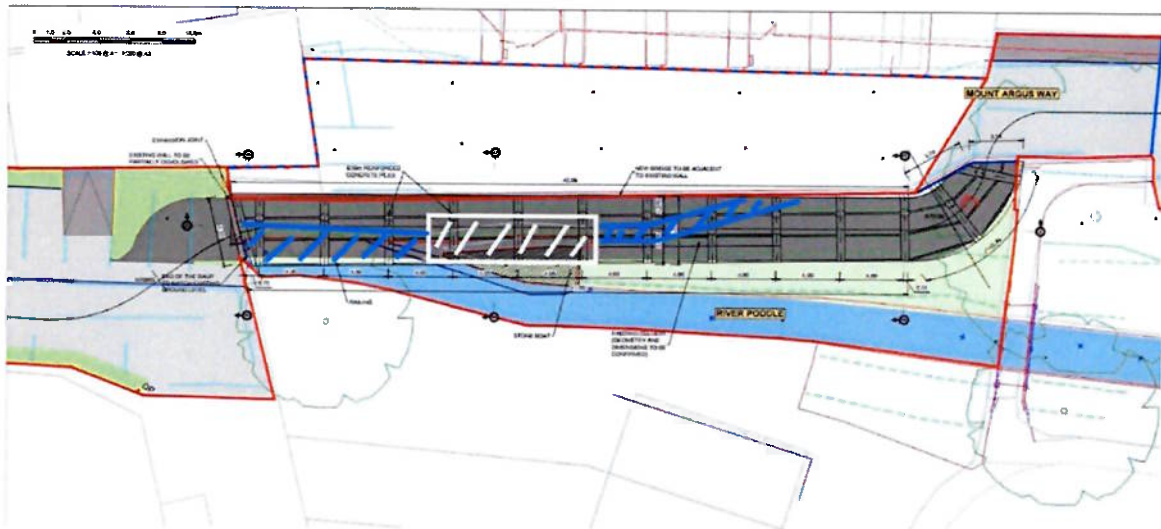


Figure 5. Plan of the boardwalk with line of the water channels indicated in blue and the area of the Tongue highlighted in white. Consideration should be given to marking these in the boardwalk design through glass panels or differential decking material.

Finally, it is not clear if any alternate designs for the boardwalk were considered. A shorter boardwalk not running over the top of the monument appears to be possible, although this would require running the cycle route through Mount Argus Square.

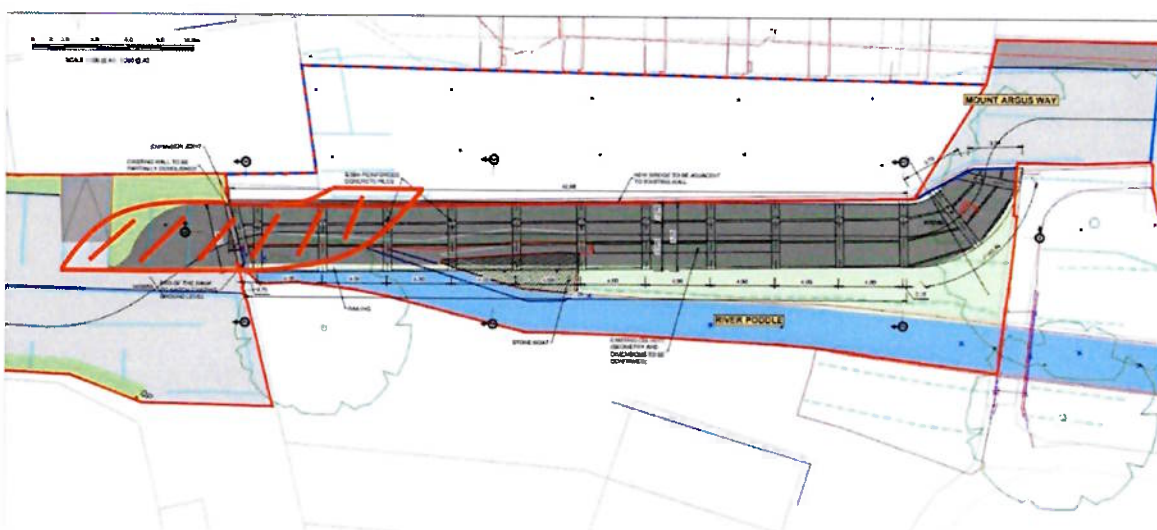


Figure 6. Potential shorter boardwalk design, allowing unobstructed viewing of the Tongue and the spilt watercourse. This would require cycle route to run through Mount Argus Square and removal of parking on street.

### Robert Emmett Bridge

Built 1935-36 to replace the earlier Clanbrassil Bridge, Robert Emmett Bridge spans the Grand Canal. It is recorded on both DCIHR (DCIHR18-15-009) and on the National Inventory of Architectural Heritage (NIAH 50080983). Flanking pedestrian footbridges proposed on both sides of the bridge will alter its setting, and it is also proposed to remove sections of historic walling. The footbridge on the west side also requires land purchase and demolition of a house. It is stated that there will be a *“Direct, Negative, Significant and Long-Term”* impact due to the proposed works in the construction phase and an *“Indirect, Negative, Moderate, Long-Term”* impact during the operational phase on the bridge, with the particular impact on the historic walls listed as *“Significant”*.

The new design will mean that traffic across the existing bridge will be limited to vehicles and a southbound cycle lane, with pedestrian traffic diverted to the boardwalks. This will prevent viewing of the Robert Emmet Memorial located on the eastern side of the bridge, and could lead to safety issues as pedestrians wishing to view the memorial may enter the carriageway and walk down the cycle lane in order to do so.



*Figure 7. Robert Emmett Memorial on bridge. Under the proposed scheme design, people wishing to view the memorial will have to enter the carriageway and stand in a cycle lane, creating a safety issue.*

It is recommended that this section of the route be considered for redesign with a view to reducing the impact on the visual setting of the bridge, retaining historic walling, and allowing pedestrian access to the Robert Emmett memorial safely.

#### **4.8.1 Recommended Conditions**

See Appendix.

#### **4.9 Parks, Biodiversity and Landscape Services**

Parks are not supportive of a proposal that provides a route through Ravensdale (Poddle) & Mt. Argus Park's for commuting cyclists whose speed will be at odds with the public using the Park's. In addition is noted that the Park is locked at night. There is lack of detail relating to the width of the proposed cycle path within both parks but the current footpath width of 2m will be insufficient for a shared scheme and will necessitate constructing a new two way cycle-way to avoid conflict between cyclists and pedestrians. This is depicted on the drawings however there is little detail given as to the width. The construction of a two way cycle path through the root zone of the existing trees will very



likely cause damage to the trees that line the path which have an important biodiversity value. Planning permission has already been granted for the incorporation of a Flood Alleviation scheme in Ravensdale (Poddle) park. The combined effect of the flood alleviation scheme and the proposed cycleway will result in further fragmentation, damage to the existing tree canopy and alter the character of the park to its detriment.

A suggested solution would be for cyclists to remain on the Kimmage Road Lower rather than detour through the park – for cyclists travelling to city centre/Kimmage village this is the most direct route and will link up with the proposed cycle track which is on road from Ravensdale Park junction.

The proposed cycle track through Mt Argus Park appears to involve the construction of a new bridge and travel through an area of trees that have not been surveyed. The impact on these trees of the new two way cycle path and bridge construction should be clarified.

The removal of the footway to the South of Harolds Cross Park along Harold's Cross Road will compromise the pedestrian comfort for people entering and exiting the park, the gate nearest Kimmage Road Lower side will be compromised while the main entrance will now result in pedestrians having a reduced landing space. Generally Pedestrian circulation is now been forced through a park which is not desirable especially at quiet times of the day or at times of darkness with the associated lack of natural surveillance. Crucially no tree survey has been submitted along here and it is not covered adequately in the Landscape Visual Impact Assessment. It's likely these mature trees along with the historic railings will be compromised. Unfortunately pedestrian comfort and safety is being reduced in order to facilitate greater vehicle movement in what is an urban village setting. We would recommend that the road is retained as is.

#### General GA Plans and Landscape Plans Comments

- The Plans issued to ABP are at a Scale of 1:500. It is very difficult to read these plans in particular their impact on existing footways and soft landscape areas; as a result these comments need to be viewed within that context.
- The plans as per previous issues appear more like concept or outline plans and not to the detail you would expect for planning.
- There are no dimensions and no sections with before and after levels and build ups etc. These would not pass our Part 8 requirements and would unlikely be deemed acceptable for a standard planning application.
- There is a real shortage of detail on the plans which would be required to make an informed opinion on them, this includes a lack of clarity where footways and kerbs are getting reduced or widened, no street lighting or signage is shown on the GA or Landscape Plans, we have no knowledge of service runs, utility cabinets or other street fixings which would impact on pedestrian comfort, safety, trees and ability to install green infrastructure in future.

#### General comments

1. Agreement on taking in charge of landscape components of the proposals by DCC Park Services will require liaison and agreement on layout and planting details prior to implementation. Amendments to planting proposals may be required to comply with Park Services contract maintenance requirements.



2. All landscape components of the proposals will require contract maintenance for a minimum of 3 years following completion of the works or any work phases. Landscape areas agreed to be taken in charge will require full completion of the 3 year maintenance period and rectification of any defects or loss of planting.
3. Tree risk assessment is to be carried out at end of year 1 and year 3 following completion of the works to identify any potential risks arising to the public or property, on retained trees within the project areas. Remedial works to trees shall be completed prior to taking in charge.
4. Tree planting proposals are welcome within the proposals, however the constraints of overhead and underground services on this planting and other constraints, such as planting close to existing boundaries should be realistically assessed at detail design stage.
5. Clarity is required on the proposed quantity of compensatory public street tree planting along the route in comparison to the proposed removal of existing street trees. If appropriate compensatory planting falls below the loss of existing street trees then other forms of compensation shall be agreed with DCC Park Services.
6. The implementation of the arboricultural and landscape proposals will require the professional input of Arboriculturists and Landscape Architects appointed by the project proponent for the full duration of project work contracts. Construction details for components of the project that will be taken in charge shall be agreed at design stage with Park Services.
7. Tree protection measures for all existing trees shall be put in place prior to the commencement of development or phases of development. Where existing trees require remedial measures to alleviate risk to the users of the completed development, then these shall be completed by the project proponent. The project proponent shall be required to fund tree remedial works or tree replacement works where existing trees retained decline or fail within 5 years of the completion of the project or project phases.

Specific comments:

Preliminary Design Report Appendix B5: Landscaping General Arrangement

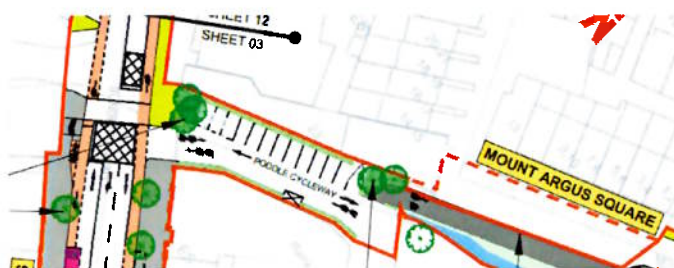
Sheet 1&2

Kimmage Road Lower: Additional street tree planting welcomed.

Sheet 3:

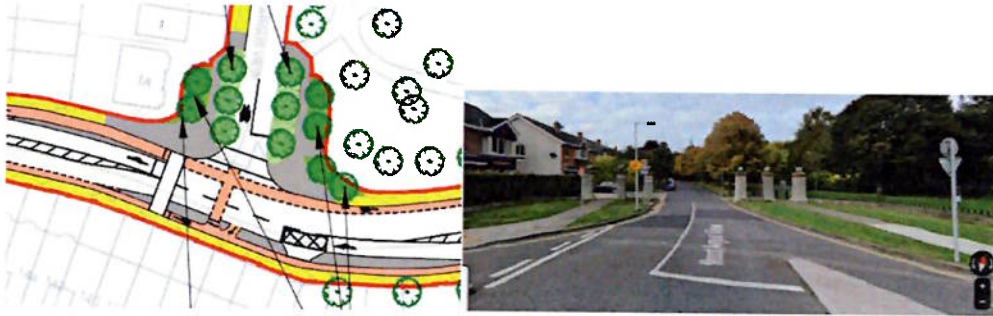
Replace Prunus species with longer life tree species.

The potential to daylight the short section of the Poddle River should be an objective rather than retaining car parking at the 'Poddle Cycleway' area, below:



Proposals of tree planting on existing river culvert should be reviewed.

Sheet 4: To maintain the visually open quality at Mount Argus View entrance (below) the new tree planting shall be reduced to four fastigate oaks on the outer grass margins.

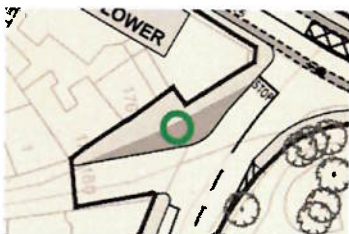


The visual quality of the Mount Argus entrance (below) would be improved by the introduction of natural stone paving, rather than extensive areas of concrete and by a reduction in the proposed car parking spaces on either side of the entrance. The existing bicycle stands should also be retained or repositioned in the scheme.



Sheet 6:

The potential for a single street tree planting on the widened footpath section (below) should be considered.

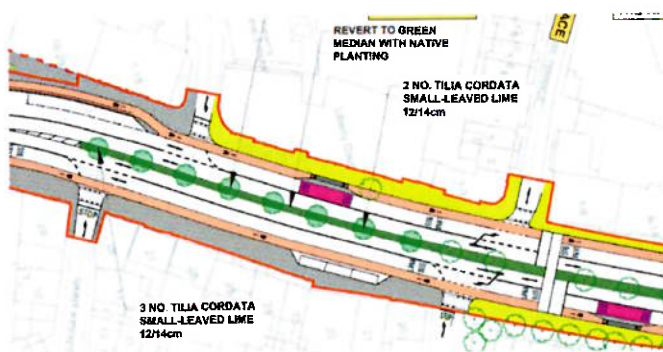


The plan (below) shall indicate the existing stone cross and its base to be retained. The surfacing at this pedestrian island should be completed in natural stone to enhance the quality of the cross area.



Sheet 9:

The proposed central median planting is welcomed. Please review species varieties for compact canopy forms due to traffic flow on both sides.



Sheet 11:

Malpas St junction area. There is a likely high impact on existing tree root zone at proposed parking bays (below 2<sup>nd</sup> tree from RHS).



#### 4.9.2 Recommended Conditions

See Appendix.

#### 4.10 City Architect's Division

The City Architects Division welcomes in principal the objectives of the Proposed Scheme to support integrated sustainable transport use through infrastructure improvements for active travel (both walking and cycling), and the provision of enhanced bus priority measures. The Proposed Scheme will facilitate the modal shift from car dependency through the provision of walking, cycle, and bus infrastructure enhancements thereby contributing to an efficient, integrated transport system and facilitating a shift to a low carbon and climate resilient city.

The Scheme notes that proposals for public realm upgrades, including widened footpaths, high quality hard and soft landscaping to contribute towards a safer, more attractive environment for pedestrians are included, and that it has been developed having regard to relevant accessibility guidance and universal design principles so as to provide access for all users.

The City Architects Division wishes to comment on the proposals, noting the following:

- The design of the public realm will be fundamental to the success of the Proposed Scheme.
- This design needs to be supported by pedestrian traffic counts to ensure that footpaths are of sufficient width to safely accommodate anticipated pedestrian volumes and to provide for ancillary public realm infrastructure such as tree-planting, greening and street furniture, as well as traffic infrastructure such as bus shelters, utility cabinets, and cycle stands etc. Footpaths should be designed to be universally accessible and pedestrian environments enhanced.
- As part of the proposals, all historic fabric and features should be retained and protected, and the settings of protected structures and buildings within Architectural Conservation Areas (ACA's) should be respected insofar as possible within the Proposed Scheme.
- Generally, existing survey drawings are submitted with a project to facilitate analysis and the extent of intervention in a proposal. The General Arrangement Drawings submitted as part of the National Transport Authority's Kimmage to City Centre Core Bus Corridor Scheme are drawn at a scale of 1:500@A1 and do not include an overlay of existing survey drawings.

The inclusion of an overlay of existing survey drawings onto the General Arrangement Drawings as submitted for the Kimmage to City Centre Core Bus Corridor Scheme would have facilitated a better assessment of the impacts of the proposals on the existing public realm.

This issue was raised in previous City Architects commentary.

- Comments will generally be confined to proposed physical interventions in the public realm only, with minimal or no commentary on traffic routing or modelling.
- Where drawings are referenced in the commentary, the relevant Drawing Sheet no. from Volume 3, Figures, Chapter 4.2 General Arrangement Drawings is included for ease of reference.
- Local Authority Boundaries: The boundaries between all local authorities should be indicated on all drawings where applicable.

#### Building Conservation Legislation

As this route involves works to and/or adjacent to Protected Structures their curtilage incl. Historic Fabric and within Conservation Areas, the applicant is to confirm that all works proposed must comply with Part IV of the Planning and Development Act 2000. This includes guidelines under S.52 (1) for the protection of structures, or parts of structures, and the preservation of the character of architectural conservation areas.

For reference, in December 2004, the Minister for the Environment, Heritage and Local Government published guidelines under S.52 (1) entitled Architectural Heritage Protection Guidelines for Planning Authorities which incorporated the S.52 (2) guidelines as Chapter 5.

Previous commentary by the City Architects Division on the BusConnects Core Bus Corridor Scheme  
 City Architects Division previously submitted detailed comments and recommendations on the BusConnects Core Bus Corridor Scheme to the Dublin City Council BusConnects Liaison Team on the following dates:

- 25th May 2019, City Architects comments on BusConnects CBC 'preferred routes', published for Round 1 of public consultation.
- 25th February 2020, City Architects Comments on BusConnects proposals, timelines, and information required.
- 18th April 2020, City Architects Comments on BusConnects CBC 'preferred routes', published for Round 2 of public consultation.
- 7th January 2021, City Architects Comments on BusConnects CBC 'preferred routes', published for 3rd Round of public consultation,

and in addition to the following studies:

- 5th August 2020, BusConnects Junction Study of 16 CBC routes.
- 5th August 2020, BusConnects CBC Civic Spine and Civic Space Study.
- 1st October 2020, Footpath Study of Routes 13 & 7.

Commentary by the City Architects Division on the BusConnects Kimmage to City Centre Core Bus Corridor Scheme, as submitted by the National Transport Authority to An Bord Pleanála

Commentary by the City Architects Division on the Proposed Scheme is limited to a review of the following documents only contained within the Environmental Impact Assessment Report of the planning documentation:

- Volume 1, Non-technical Summary
- Volume 2, Chapter 4 Proposed Scheme Description
- Volume 3, Figures, Chapter 4.2 General Arrangement
- Volume 3, Figures, Chapter 4.4 Typical Cross Sections
- Volume 3, Figures, Chapter 4.5 Landscaping General Arrangements
- Volume 3, Figures, Chapter 4.9 Street Lighting
- Volume 3, Figures, Chapter 4.10 Junction System Design
- Volume 3, Figures, Chapter 17.2 Visualisations

<b>1.</b>	<p><b>Footpath widths &amp; alignment:</b></p> <ol style="list-style-type: none"> <li>1. The provision of footpaths designed to the minimum width may not be sufficient in areas of high pedestrian traffic particularly in urban villages and busy commercial streets e.g. the footpath on Clanbrassil St Lower (Sheet 09) and Harold's Cross Rd (Sheet 07).</li> <li>2. The removal of the footpath on Harold's Cross Rd at the northern side of the park (Sheet 06) is generally not supported. If it is not possible to retain the footpath, it is suggested that the design of the junction of Harold's Cross Rd and Kimmage Rd</li> </ol>	
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	<p>Lower could be reviewed to provide a new pedestrian crossing running north-south on Kimmage Rd Lower and into a proposed new entrance into the park.</p> <p>3. Footpath widths also need to account for congregations of passengers waiting in the vicinity of bus stops and pedestrians travelling along the footpath.</p> <ul style="list-style-type: none"> <li>• <b>By condition, confirmation is requested that pedestrian traffic counts have been undertaken to ensure that the proposed footpath widths along the Proposed Scheme are sufficient to cater for anticipated pedestrian volumes. This confirmation should be submitted to the planning authority prior to commencement of development.</b></li> </ul>	
2.	<p><b>Local Public Realm Improvement Schemes:</b></p> <p>1. The Proposed Scheme includes a plan drawing of proposed public realm improvements at;</p> <ul style="list-style-type: none"> <li>○ The junction of R817 Kimmage Rd Lower and Sundrive Rd (Sheet 03) and Image 4.2: Urban Realm Upgrade at the Junction of R817 Kimmage Road Lower and Sundrive Road, Chapter 4, Proposed Scheme Description.</li> <li>○ St Patrick's Court on Clanbrassil St Lower (Sheet 09).</li> </ul> <p>The information provided is insufficient to facilitate proper assessment of the proposals and additional information is required including visualisations of the proposals.</p> <ul style="list-style-type: none"> <li>• <b>By condition, detailed drawings and specifications of the proposed public realm improvement schemes shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul>	
3.	<p><b>Land Acquisition by NTA &amp; Taking in Charge:</b></p> <p>1. Where it is proposed to CPO or acquire lands as part of the Proposed Scheme, confirmation is sought as to whether ownership of these lands will be transferred to the relevant local authority or will these lands be retained by the NTA but taken in charge by the relevant local authority for maintenance purposes.</p> <ul style="list-style-type: none"> <li>• <b>By condition details of all landscaping and public realm finishes in areas where they are to be taken in charge shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul>	
4.	<p><b>Bus Shelter Design:</b></p> <p>1. Bus shelters impact on the width of footpaths and should only be proposed where there is sufficient space to physically accommodate them and passengers congregating in their vicinity. It is unclear if there is sufficient width to the footpaths in some locations where bus shelters are proposed e.g. the proposed south bound bus stop and bus shelter on Harold's Cross Rd to the west side of</p>	

	<p>Harold's Cross Park, (Sheet 06), and the proposed north bound bus stop and bus shelter on Clanbrassil St Upper, (Sheet 08)</p> <ol style="list-style-type: none"> <li>2. Where bus shelters are proposed their locations must have regard to existing building entrances.</li> <li>3. Bus shelter locations are indicated on the drawings but information on their proposed design, size and type is not provided. <ul style="list-style-type: none"> <li>• <b>By condition, full details of the design and type of each bus shelter for each location shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul> </li> <li>4. The proposed location of bus shelters in the vicinity of buildings of architectural importance and in Conservation Areas needs to be considered carefully. Bus stops only rather than bus shelters would be preferable in Conservation Areas. The vistas and settings of Protected Structures are also impacted by the proposed siting of bus shelters in their vicinity. <ul style="list-style-type: none"> <li>• <b>By condition, full details of the design and type of each bus shelter for each location along the Proposed Scheme shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul> </li> <li>5. In the interest of visual amenity and having regard to protected structures and their settings, advertisements should not be permitted on bus shelters in Architectural Conservation Areas (ACA), Conservation Areas, Residential Neighbourhoods (Conservation Areas) or Special Planning Control Schemes (SPCS). <ul style="list-style-type: none"> <li>• <b>By condition, full details of the design and type of each bus shelter for each location along the Proposed Scheme shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul> </li> </ol>	
5.	<p><b>Siting of utility cabinets and above-ground utility infrastructure:</b></p> <ol style="list-style-type: none"> <li>1. The siting of utility cabinets, poles and other above-ground utility infrastructure may have significant impacts on the space, visual impact and quality of the public realm.</li> <li>2. This issue has been a significant problem on previous transport infrastructure projects. <ul style="list-style-type: none"> <li>• <b>By condition, the siting of all utility cabinets and other above-ground utility infrastructure shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul> </li> </ol>	
6.	<p><b>On-street Parking:</b></p>	

	<p>1. The roll-out of electric charging points for electric vehicles is required if national carbon emissions plans are to be met.</p> <ul style="list-style-type: none"> <li>• <b>By condition, the NTA should engage with electrical charging operators to co-ordinate the roll out of electrical charging points to on-street parking areas as part of the works along the route of the Proposed Scheme. This shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul>	
7.	<p><b>Palette of materials:</b></p> <p>1. It is submitted that the replacement of all the existing hard landscape surfaces with new may not be required, nor may it be financially feasible or sustainable. The existing footpaths along the length of Clanbrassil St are paved with the exception of two areas of in-situ concrete footpath to both sides of the road as indicated on Sheet 08. As part of the proposals the existing in-situ concrete footpath on the west side of the road is to be retained whereas the existing in-situ concrete footpath on the east side is to be replaced with a new in-situ concrete footpath. It would be preferable if these areas of in-situ concrete footpath were upgraded to concrete paving to ensure a consistent quality and pattern of material to the footpaths along Clanbrassil St.</p> <ul style="list-style-type: none"> <li>• <b>By condition, the extent of existing hard landscape to be retained within the Proposed Scheme shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul> <p>2. Stone or concrete sett paving is proposed for the raised tables at side road entries and parking bays. All proposed materials are to be agreed and approved by Dublin City Council, Environment &amp; Transport Department.</p> <ul style="list-style-type: none"> <li>• <b>By condition, the material palette within the Proposed Scheme shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul>	
8.	<p><b>Palette of street furniture:</b></p> <p>1. A full palette of street furniture to include street lighting, bins, benches, bollards, cycle stands, wayfinding poles, digi-panels etc. and confirmation on their proposed locations is required.</p> <p>2. Confirmation is sought as to whether an identical palette is to be used for the Proposed Scheme across all the local authority administrative areas or whether each local authority (and perhaps specific urban villages) will have their own palette.</p> <p>3. Confirmation is sought as to whether there will be uniformity in the palette of street furniture across all the BusConnects Core Bus Corridor Schemes.</p> <ul style="list-style-type: none"> <li>• <b>By condition, a full palette of street furniture and their proposed locations across all the proposed BusConnects Core Bus Corridor</b></li> </ul>	



	<p align="center"><b>Schemes, shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></p>	
<p><b>9.</b></p>	<p><b>Boundary treatments:</b></p> <ol style="list-style-type: none"> <li>1. Where property boundaries along the route are to be relocated to facilitate land acquisition, the fabric in the existing boundaries should be assessed for their architectural conservation value and cultural value. <ul style="list-style-type: none"> <li>• <b>By condition, the fabric in all property boundaries which are to be relocated to facilitate land acquisition along the Proposed Scheme should be assessed for their architectural conservation value and cultural value. This assessment should be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul> </li>   <li>2. The assessment should confirm whether the fabric, which may include railings, walls etc. is suitable for repair and re-use for sustainability reasons in the new boundaries rather than replaced with new. <ul style="list-style-type: none"> <li>• <b>By condition, the fabric in all property boundaries which are to be relocated to facilitate land acquisition along the Proposed Scheme should be assessed whether it may be suitable for repair and re-use for sustainability reasons in the new boundaries rather than replaced with new. This assessment should be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul> </li> </ol>	
<p><b>10.</b></p>	<p><b>New Pedestrian &amp; Cycle bridges:</b></p> <ol style="list-style-type: none"> <li>1. <b>Emmet Bridge, Grand Canal</b> - Two new cycle - pedestrian bridge structures are proposed on either side of the existing early twentieth century protected and NIAH listed single-arch concrete 15m wide bridge over the Grand Canal. These bridges comprise a three-span 23.5m long x 6m wide steel bridge adjacent to Robert Emmet Bridge on the western side and a three-span 24.5m long x 3.5m wide steel bridge adjacent to Robert Emmet Bridge on the eastern side (Document Volume 3, Figures, Part 3, Chapter 17.2 Visualisations View 17.2.1.5, 6, 7, 8, 9 &amp;10. Volume 3, Figures, Part 1, Chapter 4.2 General Arrangement, Sheet 08).</li> </ol> <p>The following comments refer to the design of the proposed bridges and relate to their impact on the setting and significance of the historic bridge (a protected structure).</p> <p><b>Design (Additions to an Important Landmark)</b></p> <ol style="list-style-type: none"> <li>1. The proposed bridges are adjacent to a landmark bridge and protected structure and any modifications or additions should have a design quality to reflect this status. The proposal is not of the quality required in this key location.</li>   <li>2. Please provide a Design Statement for the proposed bridges that comprehensively addresses the significance of the location, its landmark status and its protected structure status.</li> </ol>	

3. The future re-development and extension of the canal towpaths on the north side of the canal should be considered and not impeded by the proposed new bridges and their structure.

4. The spaces between the bridges and the existing protected structures should be maximised, minimum 1.5m wide on each side. This is to ensure that the clarity of the design and form of the protected structure is not compromised by the location of the additions which are currently too close.

5. It is unclear from the drawings how the existing levels are being addressed.

6. The fascia of the bridges appear heavy and un-designed in the visualisations provided and should be re-considered.

7. The design quality of these bridges is very important and the bridge design should be considered for an architectural /engineering design competition.

#### **Conservation**

1. Please provide an Architectural Heritage Impact Assessment of the bridge design prepared by a qualified Building Conservation Specialist.

2. The junctions where the new bridge abuts the old bridge should follow best practice conservation principals (ref. Chapter 7 'Architectural Heritage Guidelines' A Guide for Planning Authorities'). These areas need to be redesigned.

3. There is an unacceptable 'blurring' of new and old elements where the modern stonework is built to replicate historic stonework. This approach does not follow acceptable conservation practice. These areas need to be redesigned.

4. All junctions between existing and new (including ground surfaces) should be clearly defined in order to acknowledge the historic elements. In this case no attempt at a design/conservation intent is evident. This approach does not follow acceptable conservation practice. These areas need to be redesigned.

- **By condition, revised proposals for the bridge addressing all of the above are to be submitted and agreed with DCC Planning and Conservation section prior to commencement of development.**

2. **Stoneboat Boardwalk** between Sundrive Road and Mount Argus. A 4m wide and 42m long steel boardwalk structure for pedestrians and cyclists is proposed over the western bank of the River Poddle and will be supported over the bank by 13 piers. (Document Volume 3, Figures, Part 3, Chapter 17.2 Visualisations View 17.2.1.1 & 2. Volume 3, Figures, Part 1, Chapter 4.2 General Arrangement, Sheet 03).

The following comments refer to the design of the proposed bridge/ boardwalk.

	<p>1. It is not clear from the information provided how the undercroft of the bridge will be designed so that it does not attract anti-social behaviour.</p> <ul style="list-style-type: none"> <li>• <b>By condition, further information on the design of the bridge/ boardwalk, measures to address any potential anti-social behaviour and a full landscaping proposal are to be submitted and agreed with DCC Planning department prior to commencement of development.</b></li> </ul>	
11.	<p><b>Per cent for Art Strategy:</b></p> <p>1. It is not clear where the Percent for Art Strategy is to be incorporated into this project.</p> <ul style="list-style-type: none"> <li>• <b>By condition, the selection and location of artworks along the route as part of the Percent for Art strategy shall be reviewed and agreed with the local authority Arts Office and submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul>	
12.	<p><b>Painted Medians:</b></p> <p>1. The painted median along Kimmage Rd Lower should be reviewed to provide additional greening between Larkfield Ave and Priory Rd if possible or alternatively the median space removed and reallocated into the footpaths adjacent.</p> <ul style="list-style-type: none"> <li>• <b>By condition, all painted medians should be reviewed to ascertain if they could provide additional greenery or alternatively if the median space could be removed and reallocated into the footpaths adjacent. This shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul>	
13.	<p><b>Traffic Signal &amp; Signage Poles:</b></p> <p>1. The number of poles installed to provide traffic signals for pedestrians, cyclists, buses and other vehicles needs to be rationalised to the minimum required at each junction.</p> <p><b>By condition, the number of poles required for traffic signal and signage needs to be designed to the minimum. This information shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></p>	
14.	<p><b>Public Lighting:</b></p> <p>1. Public Lighting Division, DCC, should be consulted on the locations and style of lamp standard fittings proposed as part of the Kimmage to City centre scheme. There are a number of existing heritage lamp standards along Clanbrassil St which should be retained.</p> <ul style="list-style-type: none"> <li>• <b>By condition, Public Lighting Division, DCC, are to be consulted on the locations and styles of public lamp standards in the city centre. This information shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul>	

17.	<p><b>Water Drinking Fountains:</b></p> <ol style="list-style-type: none"> <li>1. In order to reduce plastic waste and promote sustainability, a strategy for the roll-out of water drinking fountains, such as the recently installed model on Clarendon Row, should be incorporated into the Proposed Scheme at suitable locations and in consultation with Dublin City Council. <ul style="list-style-type: none"> <li>• <b>By condition, suitable locations for water drinking fountains should be identified and installed as part of the works along the route of the Proposed Scheme. This shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development</b></li> </ul> </li> </ol>	
18.	<p><b>Village Signage:</b></p> <ol style="list-style-type: none"> <li>1. Existing 'Welcome to Village xxx' signage provide local wayfinding landmarks and should be retained as part of the Proposed Scheme, in agreement with the local authority and community e.g. 'Welcome to Harold's Cross' at the entrance to Mount Argus View, Sheet 04.</li> <li>2. The Proposed Scheme presents an opportunity to implement a scheme of city wide co-ordinated village signage, in collaboration with the relevant local authority and Area Offices, as part of the overall Bus Connects Core Bus Corridor Project. <ul style="list-style-type: none"> <li>• <b>By condition, village signage should be incorporated into the Proposed Scheme, and a scheme of city wide co-ordinated village signage should be undertaken and shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.</b></li> </ul> </li> </ol>	

#### 4.10.2 Recommended Conditions

See Appendix.

#### 4.11 Conservation Section

The following high-level architectural heritage assessment has been carried out by the Conservation Section in the context of the Dublin City Development Plan 2022 – 2028, other key policy documents and best conservation practice. It is submitted that the following policies and provisions in particular should be taken into account in the consideration of all proposed routes and their impacts on the architectural and built heritage of the city:

##### Dublin City Development Plan 2022 – 2028

Chapter 11 – Built Heritage and Archaeology, 11.1 Introduction, *'It is recognised that the city's built heritage contributes significantly to the collective memory of its communities and to the richness and diversity of its urban fabric. It is key to the city's character, identity and authenticity and vital social, cultural, and economic asset for the development of the city.'*

*The city's historic buildings, streetscape villages, Georgian terraces and squares, Victorian and Edwardian architecture, industrial heritage, institutional landmarks, modernist buildings of the 20th*

century, urban core and the Medieval City, together with its upstanding monuments and buried archaeology contribute to its local distinctiveness and help create a strong sense of place for citizens and visitors to the city and its neighbourhoods.'

**Section 11.5.1 Curtilage of a Protected Structure** states 'The curtilage of a protected structure is often an essential part of the structure's special interest. In certain circumstances, the curtilage may comprise a clearly defined garden or grounds, which may have been laid out to complement the design or function.'

It is the Policy of Dublin City Council:

**BHA2: Regarding Development of Protected Structures:**

'That development will conserve and enhance Protected Structures and their curtilage and will:

- a) Ensure that any development proposals to protected structures, their curtilage and setting shall have regard to the Architectural Heritage Protection Guidelines for Planning Authorities (2011) published by the Department of Culture, Heritage and the Gaeltacht.
- b) Protect structures included on the RPS from any works that would negatively impact their special character and appearance.
- e) Ensure that the form and structural integrity of the protected structure is retained in any development and ensure that new development does not adversely impact the curtilage or the special character of the Protected Structure.
- h) Protect and retain important elements of built heritage including historic gardens, stone walls, entrance gates and piers and any other associated curtilage features.
- i) Ensure historic landscapes, gardens and trees (in good condition) associated with the protected structures are protected from inappropriate development.'

**Section 11.5.2 Architectural Conservation Areas and Conservation Areas** states: 'The Planning and Development Act, 2000 (as amended), provides the legislative basis for the protection of Architectural Conservation Areas (ACAs). Under the Act, an ACA is defined as a place, area, group of structures or townscape that is of special architectural, historical, archaeological, artistic, cultural, scientific, technical, social interest or value or contributes to the appreciation of protected structures.'

Architectural Conservation Areas (ACAs) are designated in recognition of their special interest or unique historic and architectural character and important contribution to the heritage of the city. This character is often derived from the cumulative impact of the area's buildings, their setting, landscape and other locally important features which developed gradually over time...

The protected status afforded by inclusion in an ACA only applies to the exterior of structures and features of the streetscape.'

It is the Policy of Dublin City Council:

**BHA7: Regarding Architectural Conservation Areas:**

- a) To protect the special interest and character of all areas which have been designated Architectural Conservation Areas (ACA). Development within or affecting an ACA must contribute positively to its character and distinctiveness, and take opportunities to protect and enhance the character and appearance of the area and its setting, wherever possible. Development shall not harm buildings,

*spaces, original street patterns, archaeological sites, historic boundaries or other features, which contribute positively to the special interest of the ACA.*

*d) Seek the retention of all features that contribute to the character of an ACA including boundary walls, railings, soft landscaping, traditional paving and street furniture.*

*'All trees which contribute to the character and appearance of an Architectural Conservation Area, in the public realm, will be safeguarded, except where the tree is a threat to public safety, prevents universal access or requires removal to protect other specimens from disease.'*

*It is the Policy of Dublin City Council:*

**BHA8: Regarding Demolition in an ACA:**

*'There is a presumption against the demolition or substantial loss of a structure that positively contributes to the character of the ACA except in exceptional circumstances where such loss would also contribute to a significant public benefit.'*

**Section 11.5.3 Z2 and Z8 Zonings and Re-Hatched Conservation Areas**

*'The Z8 Georgian Conservation Areas, Z2 Residential Conservation Areas and red-lined Conservation Areas are extensive throughout the city. Whilst these areas do not have a statutory basis in the same manner as protected structures or ACAs, they are recognised as areas that have conservation merit and importance and warrant protection through zoning and policy application.*

*...The special interest/value of Conservation Areas lies in the historic and architectural interest and the design and scale of these areas. Therefore, all of these areas require special care in terms of development proposals. The City Council will encourage development which enhances the setting and character of Conservation Areas.*

*As with Architectural Conservation Areas, there is a general presumption against the development which would involve the loss of a building of conservation or historic merit within the Conservation Areas or that contributes to the overall setting, character and streetscape of the Conservation Area. Such proposals will require detailed justification from a viability, heritage and sustainability perspective.'*

*It is the Policy of Dublin City Council:*

**BHA9: Regarding Conservation Areas, enhancement opportunities may include:**

*'3. Improvement of open spaces and wider public realm and reinstatement of historic routes and characteristic plot patterns.'*

*It is the Policy of Dublin City Council:*

**BHA10: Regarding Demolition in a Conservation Area:**

*'There is a presumption against the demolition or substantial loss of a structure that positively contributes to the character of the Conservation Area, except in exceptional circumstances where such loss would also contribute to a significant public benefit.'*

*It is the Policy of Dublin City Council:*

**BHA15: Regarding Twentieth Century Buildings and Structures:**

a) *'To encourage the appropriate development of exemplar twentieth century buildings and structures to ensure their character is not compromised.'*

*It is the Policy of Dublin City Council:*

**BHA16: Regarding Industrial Heritage:**

a) *'To have regard to the city's industrial heritage and Dublin City Industrial Heritage Record (DCHIR) in the preparation of Local Area Plans and the assessment of planning applications...'*

### **Section 11.5.3 Protection of Historic Ground Surfaces, Street Furniture and Public Realm**

*'Dublin is fortunate to still retain impressive areas of historic street surfaces such as granite kerbing, granite pavement flags and granite and/or diorite setts, mainly but not entirely situated in the city centre. These along with other important historic features in the public realm such as milestones, city ward stones, street furniture, water troughs, post boxes, lampposts and railings make a special contribution to our built heritage. These items are often an integral part of the urban landscape or province significant historic references which greatly contribute to the character of an area, especially where they complement the architectural features of protected structures, Architectural Conservation Areas and Z2, Z8 and Red-Hatched Conservation Areas.'*

*It is the Policy of Dublin City Council:*

**BHA18: Regarding Historic Ground Surfaces:**

a) *'To protect, conserve and retain in situ historic elements of significance in the public realm including milestones, jostle stones, city ward stones, bollards, coal hole covers, gratings, boot scrapers, cast iron basement lights, street skylights and prisms, water troughs, street furniture, post boxes, lampposts, railings and historic ground surfaces including kerbs, pavement flags and setts and to promote conservation best practice and high standards for design, materials and workmanship in public realm improvements within areas of historic character, having regard to the national Advice Series on 'Paving: The Conservation of Historic Ground Surfaces (2015).'*

*It is the Policy of Dublin City Council:*

**BHA24: Regarding Reuse and Refurbishment of Historic Buildings:**

*'Dublin City Council will positively encourage and facilitate the careful refurbishment of the historic built environment for sustainable and economically viable uses and support the implementation of the National Policy on Architecture as it relates to historic buildings, streetscapes, towns and villages, by ensuring the delivery of high quality architecture and quality place-making and by demonstrating best practice in the care and maintenance of historic properties in public ownership.'*

*It is the Policy of Dublin City Council:*

**BHA26: Regarding Archaeological Heritage:**

(5) *'To preserve known burial grounds and disused historic graveyards. Where disturbance of ancient or historic human remains is unavoidable, they will be excavated according to best archaeological practice and reburied or permanently curated.'*

(6) *'Preserve the character, setting and amenity of upstanding and below ground town wall defences.'*

The Conservation Section would like to highlight that trees contribute significantly to the streetscape and character of the historic areas of the city, including the character and setting of Protected Structures, Architectural Conservation Areas and 'red-hatched' Conservation Areas, as provided in the Dublin City Development Plan 2022-2028.

As noted in the Dublin City Tree Strategy 2016 – 2020, *'Dublin City's identity is expressed in a pattern of tree lined streets and open spaces. Trees form an integral part of the urban fabric of Dublin City whether they are in public or private ownership... Trees contribute to urban design and can help define spaces... They can also create areas of particular urban character and ambience as the use of the term Dublin's leafy suburbs suggests and they provide a verdant frame for our historic buildings.'* Section 3.6.1 *'Private trees whether in gardens, residential or business premises make a significant contribution to the visual amenity of Dublin City and provide an important habitat for wildlife. They may act as landmarks, identify a particular location, provide a foil to the urban townscape and impart a sense of character to the area...'*

The Conservation Section recommends that all mature and historic trees across the Bus Connects proposal and particular in close proximity to Protected Structures and within ACAs, Conservation Areas and areas zoned Z2 and Z8 in the Dublin City Council Development Plan 2022-2028 are retained and protected as far as practically possible. Where there is an unavoidable loss of historic trees, the NTA shall ensure that these are replaced with new semi mature trees to the satisfaction of DCC.

#### **Architectural Heritage Protection Guidelines for Planning Authorities (2011)**

Consideration of proposals affecting boundary features:

**13.4.3** *'Proposals to remove or alter boundary features could adversely affect the character of the Protected Structure and the designed landscape around it... such alterations can have a detrimental effect on the character of a Protected Structure and on the character of an ACA.'*

**13.4.4** *'...the cumulative effect on the character of the street or area of a series of incremental changes may not be acceptable.'*

**p.197** *'...Gardens are generally a combination of built features and planting. Regardless of its size, a garden can make an important contribution to the character and setting of a Protected Structure...'*

#### **14.4.1 Street Furniture and Paving**

*'An item of street furniture may be protected by being included in the RPS in its own right where it is special or rare; as part of the curtilage of a Protected Structure; or as part of an ACA. Such items could include lamp standards, seats and benches, bollards, railings, street signs, iron signposts, free standing or wall mounted post boxes, telephone kiosks, horse troughs, water pumps, drinking fountains, jostle stones, milestones, paving, kerbstones, cobbles and setts, pavement lights, coal hole covers, weighbridges, statues and other monuments.'*

**Department of Culture, Heritage and the Gaeltacht – Technical Advice Series**



The Technical Advice Documents on Paving – *the conservation of historic ground surfaces and Iron – the repair of wrought and cast ironwork* should be used to guide any interventions to historic boundary railings and paving arising from the proposed works.

### **Assessment**

The potential impact of the proposed development on the architectural heritage of this route and on the following categories in particular, has been subject to a relatively high-level assessment:

- Protected Structures and Proposed Protected Structures and their settings
- Buildings and other structures (post boxes/milestones etc.) and historic landscapes included on the National Inventory of Architectural Heritage (NIAH)
- Structures included in the Dublin City Industrial Heritage Record Survey (DCIHR)
- Other unprotected structures that contribute positively to the architectural heritage and character of streetscapes
- Architectural Conservation Areas (ACAs) and Conservation Areas
- Lands zoned Z2 in the Dublin City Development Plan 2022-2028, which aims to ‘protect and / or improve the amenities of residential conservation areas’
- Lands zoned Z8 in the Dublin City Development Plan 2022-2028, which aims ‘to protect the existing architectural and civic design character, and to allow only for limited expansion consistent with the conservation objective’
- Historic Paving and Kerbing

### **General Response**

The Conservation Section finds that a thorough study of the receiving environment has been carried out. The assessment of architectural heritage, streetscape and the urban environment submitted as part of the EIAR and the proposed mitigation measures across the scheme is generally welcomed.

Appendix A16.1 Historical Background provides a detailed and well-researched discussion on the history of the development of the route. Appendix A16.2 Inventory of Architectural Heritage Sites provides a written and photographic record, importance rating and sensitivity rating for all protected structures, NIAH-recorded structures, designed landscapes, unprotected structures of built heritage significance, street furniture, paving and surface treatments. The record is comprehensive and accurately describes the quality and status of the heritage structures along the proposed route. Appendix A16.3 Methodology for Works Affecting Sensitive and Historic Fabric, Section 3.2.1 provides a statement on protection of architectural heritage structures and features during works. *‘Some architectural heritage features will require protection during the course of works, where there is potential for damage of sensitive fabric during the course of works proposed in close proximity to them.*

*Historic or sensitive fabric will be recorded in position prior to the commencement of construction works, protected, and monitored for the duration. Appropriate protections will be determined depending on the nature of the fabric and the construction activities. Protective measures will include cordoning off as appropriate and/or the provision of protective wrapping or temporary hoardings or boxing off. More specific protections are outlined in the relevant sections of this methodology’.*

In general, Dublin City Council's Conservation Section agrees with the above findings regarding mitigation and protection measures.

### Key Impacts

Having regard to the information submitted the following are considered by the Conservation Section to be the key impacts of the Kimmage to City Centre route in relation to architectural heritage:

- Protected Structures and their settings
  - a) Several Protected Structures are included on the subject map sheets. These structures are located on or adjacent to the route boundary. All Protected Structures in close proximity to construction works are to be adequately protected and all proximate works are to be supervised by a conservation professional including 7-13 Clanbrassil Street Upper (RPS 1858-1864), 14-20 Clanbrassil Street Upper (RPS 1865-1871), 50-55 Clanbrassil Street Upper (RPS 1872-1877), 89, 91, 93, 95, 97, 99, 101, 103, 105, 107 South Circular Road (RPS 1827-1836), 119, 121, 123, 125, 126, 127, 128, 130 and 132 South Circular Road (RPS 1837-1845), 29 Clanbrassil Street Lower (RPS 1857), 21 New Street South (RPS 5823), and Granite base to former public lavatories on New Street South (RPS 5822), 1-3 Waverley Terrace (RPS 8333-8335), 23-27 Kenilworth Square North (RPS 4120-4124).
  - b) A bus stop and shelter are proposed outside the northern boundary wall of 27 Kenilworth Square North (RPS 4124).
  - c) A bus stop and shelter are proposed outside the front boundary of 16-17 Clanbrassil Street Upper (RPS 1867-1868).
  - d) Revised parking arrangement at 21 New Street South (RPS 5823).
  - e) Granite base to former public lavatories on New Street South (RPS 5822/NIAH 50080678) is located within the site boundary line. The landscape drawings indicated that conservation works are proposed at this location, but no further information is provided. Details of any conservation works proposed at this site should be submitted to the Planning Authority for approval.
- NIAH Structures and their settings
  - a) All NIAH Structures in close proximity to construction works are to be adequately protected and all proximate works are to be supervised by a conservation professional including works proximal to 72/74 Harold's Cross Road (NIAH50081059), 7-8 Clanbrassil Street Upper/Wesley Place (NIAH 50080940), 12-13 Clanbrassil Street Upper/Wesley Place/Wesley Place (NIAH 50080941), 18-20 Clanbrassil Street Upper (NIAH 50080984), 50-55 Clanbrassil Street Upper (NIAH 50080987), Post office 65-66 Clanbrassil Street Upper (NIAH 50080943), Leonard's Corner Post Office 67-68 Clanbrassil Street Upper/109 South Circular Road (NIAH 50080943), 87-107 South Circular Road (NIAH 50080946), 119, South Circular Road (NIAH 50080864), 121/123/125/127, South Circular Road (NIAH 50080865), 126/128/130/132, South Circular Road (NIAH 50080862), 29, Clanbrassil Street Lower (NIAH 50080888), Atkinson House, 21, New Street South (NIAH 50080677), Public convenience, Kevin Street Upper/New Street South (NIAH 50080678).
  - b) New trees are proposed outside the Sisters of Saint Clare's Convent (NIAH 50081054) and Chapel (NIAH 50081053), Harold's Cross Road.
  - c) A new signal controlled priority is proposed 75/77 Harold's Cross Road (NIAH 50081052)
  - d) A car park is proposed to the north of 66/68/70 Harold's Cross Road (NIAH 50081060)
  - e) A new bus stop and shelter is proposed at 14-17 Clanbrassil Street Upper/Wesley Place (NIAH 50080985)

- f) A post box Clanbrassil Street Upper (NIAH 50080944) is located within the site boundary line.
- g) Robert Emmet Bridge (NIAH 50080983) Clanbrassil Street Upper will be directly and indirectly impacted by the proposed scheme. A new independent cycle/pedestrian bridge is proposed to the west side of the bridge. The cycle/pedestrian bridge will be 24m in length and 6m in width and will include glass panels to provide edge protection. A section of the existing parapet wall adjacent to Parnell Road will be removed to allow access onto the cycle/pedestrian bridge.

A new pedestrian bridge is proposed to the east side of the bridge. The new pedestrian bridge will be 25m in length and 3m in width. A section of the retaining wall adjacent to Grove Road/Windsor Terrace will be demolished to facilitate construction. An ancillary ramp structure is proposed on the north-eastern side of the bridge at Windsor Terrace.

The decks of both bridges are proposed to be perforated steel support on steel beams on reinforced concrete abutments and intermediate steel piers. The abutments and piers are proposed to be piled into the tow path or canal banks to the north and south sides of the bridge, impacting the use of the tow path in the future.

Photomontages (Figure 17.2.1.5.-17.2.1.10) indicate the proposed design of the new structure. The new construction is considered to be utilitarian in design and of low architectural quality and would have a serious negative visual impact on the existing bridge structure, impacting negatively on the legibility of the bridge arch from the east and west sides.

The applicant is therefore requested to fully reconsider the design of the scheme at this location to improve the architectural quality of the submission and to lessen the impact of the new bridge structures on the special architectural character and legibility of the historic bridge.

New work and materials should be of the highest possible architectural quality and should complement the historic features of the bridge. Where new materials are introduced these should be legible as a new intervention.

A full architectural heritage impact assessment shall be carried out by a suitably qualified conservation professional for the new bridge elements, providing detailed design information and outlining the nature and likely impacts of the proposed bridge structures.

- h) The historic masonry retaining walls (NIAH 50080982) on the west side of Clanbrassil Street Upper will be directly impacted by the proposed scheme. The lower wall is proposed to be demolished and the upper wall buried behind new fill material to facilitate road widening to the north of Robert Emmet Bridge. A new retaining wall is proposed to be built in their place. This is poor conservation practice. Associated works will involve the demolition of a dwelling at Gordon's Fuels.

The Photomontages (Figure 17.2.1.7-17.2.1.10) indicate that the replacement wall is proposed to be in squared limestone masonry similar to the fabric of the historic wall. Best conservation practice would recommend that as far as practically possible, original material is reused in the construction of new work.

The Section submits that proposed interventions to the historic walls (which lead up to and form the setting of Robert Emmet Bridge) would result in significant loss of historic fabric and would cause serious injury to the historic fabric and should therefore be reconsidered.

The concealment / burial of historic walls is not appropriate. The applicant is requested to fully reconsider the design of the scheme at this location to lessen the impact on the historic walls. The CO recommends that in lieu of burial, the maximum amount of surviving historic fabric is carefully lifted and reinstated in its new location in accordance with best conservation principles. The historic fabric shall be recorded prior to removal, catalogued and numbered to allow for authentic re-instatement.

Where new work is proposed, new work and materials should be of the highest possible architectural quality and should complement the historic features of the wall.

A full architectural heritage impact assessment by a suitably qualified conservation professional is required for the new retaining wall, providing detailed design information and outlining the nature and likely impacts of the proposed works.

- Architectural Conservation Areas

The proposed route does not run through any Architectural Conservation Areas identified in the Dublin City Development Plan 2022-2028. The proposed scheme runs through Harold's Cross which has been identified as a priority ACA project during the lifetime of the current development plan.

- Conservation Areas, Z2 and Z8 Zonings

- a) The route will cross a red hatch Conservation Area at the Grand Canal. Robert Emmet Bridge is located within the red hatch Conservation Area. The scheme maps indicated a proposed new pedestrian/cyclist bridges on the east and west sides of the bridge and significant land acquisition with the red hatch Conservation Area.
- b) The route will run along the boundaries of a number of areas zoned Z2 including Terenure Road West, Poddle Park, Kimmage Road Lower (which includes a proposed new bus stop and shelter at 72/74/76 Kimmage Road Lower); Harold's Cross Road, Clanbrassil Street Upper (which includes a proposed new bus stop and shelter at 15-17 Clanbrassil Street Upper); South Circular Road; and Malpas Place (where bus stop is to be removed and relocated).
- c) The route will not run along the boundaries of any areas zoned Z8.

- Industrial Heritage Sites

- a) A review of the DCIHR (DCC 2003 to 2009) revealed that there are four Industrial Heritage sites identified in the study area. These include sections of former tramway at Harold's Cross Road and Clanbrassil Street Lower (DCIHR 18\_15\_030). Infrastructure associated with the tramway may survive below the road surface. St. Kevin's Hall on Clanbrassil Street Upper is located on the site of a former weaving mill (DCIHR 18\_11\_100). The route crosses Robert Emmet Bridge, a canal bridge over the Grand Canal. The construction of new independent structures to the east and west of the bridge to accommodate new pedestrian/cyclist bridges should into account the industrial significance of the bridge. Any historic fabric found

beneath the surface of the road should be carefully labelled, protected, stored, repaired and reinstated in a meaningful manner within the scheme.

- Potential impacts on historic paving and kerbing, historic street furniture and lamp standards and other features:
  - a) Historic Paving and Granite Kerbing:
    - Narrow granite kerbs to Lower Kimmage Road (CBC0011BTH129, CBC0011BTH120, CBC0011BTH140)
    - Broad and narrow granite kerbs, Rathgar Avenue (CBC0011BTH130)
    - Granite kerbs 182-194 Harold's Cross Road (CBC0011BTH127, CBC0011BTH119), St. Clare's Convent Harold's Cross Road (CBC0011BTH128), west side Harold's Cross Road (CBC0011BTH123);
    - Granite kerbing at 1, 3, 5, 7, 9, 11, 13, 15 Harold's Cross Road (CBC0011BTH167) will be repositioned as part of the scheme.
    - Cobbled surface at entrance to Harold's Cross Green (CBC0011BTH199)
    - Wide granite kerbing to east and west sides of Robert Emmet Bridge, likely salvaged from late 18th century canal bridge (CBC0011BTH135) will be repositioned as part of the scheme.
    - Narrow granite kerbs 40-68 Clanbrassil Street Upper (CBC0011BTH124)
    - Diorite kerbing 35-45 Clanbrassil Street Lower (CBC0011BTH125)
    - Cobbled surface to laneway to 20 Clanbrassil Street Upper (CBC0011BTH204)
    - Cellar hatches 1, 3, 5, 7, 9, 11, 13, 15 Harold's Cross Road (CBC0011BTH164, CBC0011BTH165, CBC0011BTH166, CBC0011BTH162, CBC0011BTH163, CBC0011BTH160, CBC0011BTH161, CBC0011BTH159, CBC0011BTH157, CBC0011BTH158)
    - Cellar hatches 34-35 Clanbrassil Street Upper (CBC0011BTH169, CBC0011BTH170)
  - b) Lamp Posts and Tram Standards:
    - There are groups of historic lamp posts along the route at Kenilworth Square North (CBC0011LP034), Kenilworth Square West (CBC0011LP035-39), Kenilworth Park (CBC0011LP026-33), Waverly Terrace (CBC0012LP027-28), and Harold's Cross Road (CBC1012LP118).
    - There is a historic tram standards at Harold's Cross Road (CBC1012LP118).
    - The historic tram standard at Zuma Terrace (CBC1011LP040) will be removed during construction works associated with the scheme and subsequently replaced.
    - There are reproduction lamp standards at Clanbrassil Street Lower (CBC0011LP001 to CBC0011LP018) and New Street South (CBC0011LP019 to CBC0011LP032, CBC0809LP031). The majority of these will require removal during construction works associated with the scheme and will be subsequently replaced.
    - The remaining lamp posts and standards will be retained in situ.
    - There is the potential for all heritage lamp posts to be damaged during the construction phase of the scheme. Exact details of the relocation of the historic lamp posts must be provided and the works should be overseen by a suitably qualified conservation professional.
  - c) Milestones:
    - There are no milestones recorded within DCC's statutory area.
  - d) Other Street Furniture/Finishes:

- War memorial cross to north end of Harold's Cross Park is located within the site boundary.
  - A historic post box at Sundrive Road is located within the scheme boundary and will be relocated; a wall mounted post box at the entrance to Mount Argus is within scheme boundary; and a historic post box at the junction between Clanbrassil Street Lower and Harty Place is located within the scheme boundary.
- e) Proposed Tree Removal
- The Tree Removal plan which forms part of the Arboricultural Impact Assessment was reviewed for impacts on significant elements of built heritage. No trees or groups of trees that impact on the settings of protected structures will be removed. The removal of two trees and their replacement by four new trees is proposed at St. Clare's Convent (NIAH 50081054), and removal of two trees at Robert Emmet Bridge (NIAH 50080983), may have an impact on the surrounding built heritage.
- Boundary Treatments
    - a) The Conservation Section notes that where works may require the removal of existing roadside boundary walls, railings, entrances gates and hedgerows, together with areas of existing garden plantings garden trees, paving and garden features, new boundary walls, railings, entrances gates and hedgerows to match existing shall be reinstated at setback location, pending agreement on more detailed design with the Planning Authority's Conservation Section and having regard to the provisions of the Architectural Heritage Protection Guidelines for Planning authorities (2011) and the relevant DHLGH Advice Series publication(s).
  - Cycle Lanes
    - a) The Conservation Section request that where the cycle ways are located in close proximity to Protected Structures and within Architectural Conservation Areas generally, that an alternative high quality cycle lane surface is provided in-lieu of red tarmacadam.
  - New Traffic Semaphore & Signage
    - a) The proposed new bus lanes and routes may require additional traffic semaphores and signage.
    - b) Careful consideration shall be given to the siting of associated utilities and traffic management signage in relation to Protected Structures and Conservation Areas, historic paving and historic street furniture and should be kept to the necessary minimum.
    - c) Consideration should be given to the rationalisation of all traffic infrastructure such as signage, traffic poles, utility boxes etc. across the route to reduce visual clutter, in particular in the vicinity of Protected Structures, within Architectural Conservation Areas, red-hatched conservation areas and in residential conservation areas.
    - d) Consideration should be given to the omission of gantry traffic signage in the vicinity of Protected Structures, within Architectural Conservation Areas, red hatched conservation areas and residential conservation areas and alternative traffic signage solutions should be sought.
  - Proposed Bus Stops
    - a) The location, form and materials of the proposed bus stops / shelters / information posts has the potential to impact upon the character and setting of Protected Structures and Conservation Areas.

- b) Mitigation will be required to mitigate the visual impact of bus stops / shelters / information posts sited near or fronting Protected Structures and Architectural Conservation Areas.
- c) The treatment of new kerbing and paving associated with the provision of bus stops / shelters / information boards should be appropriate in material and colour to the location, particularly where adjacent sections of historic stone paving and kerbing exist in situ.
- d) The alignment of footpaths should respect the setting of Protected Structures and buildings on the NIAH.

#### **4.11.2 Recommended Conditions**

See Appendix.

### **4.12 Environment and Transportation Section**

#### General Comments

The Department is generally supportive of the improvements to bus and cycling infrastructure proposed in the overall context of encouraging a shift to sustainable mobility. In this regard the proposal generally aligns with the policies expressed in the Dublin City current and draft Development Plans.

Dublin City Council is obligated to consider the Proposed Scheme in the context of the vision and range of policies set out in the current and draft development plans with a view to safeguarding the city as a place in which to live, work, visit and do business.

Dublin City Council recognises that the bus is the most important mode of public transport in Dublin and this is best illustrated by the fact that in 2019, almost 160 million journeys were made by bus in the Dublin Region, representing 65% of all public transport trips in the Dublin area. In addition, the DCC/ NTA cordon count in 2019 showed that the bus was the single highest mode of transport crossing the canal, 30% of all trips, and the bus accounted for over half of all public transport trips into the city centre.

The return of bus passenger number to above pre covid levels and the increase of Bus use at weekends of 27% over the pre covid levels is very welcome.

The commitment by the NTA within the BusConnects project to increase the level of priority afforded to the bus service is very much welcomed. The introduction of, for the most part, separated and segregated cycle ways is again welcomed as providing the opportunities:-

- To provide a better and safer cycling environment for all ages and abilities
- Help the bus maintain a steady speed and so achieve its journey times and even headways by removing bicycles from potentially being a source of delay in the bus lane.

#### Traffic Division

The Traffic Section is supportive of the integrated sustainable transport proposals and recognises the significant improvements that they will bring in terms of safe cycling measures and in enabling an efficient public transportation service along these routes.

It is essential on all BusConnects corridors to ensure that the bus service is given priority “The proposed scheme to operate on a managed headway basis”. Therefore, the corridor cannot be

operated in isolation and must in fact be a managed corridor such that the DCC traffic control system is constantly managing requests for priority and has the necessary information to determine what level of priority is appropriate in order to maintain an even headway on the corridor.

The DCC centralised traffic control system has for a number of years been linked to the bus automatic vehicle location system via a bespoke software called DPTIM and this link provides details of the bus location, its journey pattern and if the bus is ahead or behind schedule. For the BusConnects project this system is being upgraded to link to the next Generation Automatic vehicle location system which will allow finer grain information to be transmitted to the DCC system for dynamic management of the corridor.

The modelling work which was carried out on the corridor using Vissim attempted to mimic the real life operation of a full corridor management system using an adaptive traffic control system and allows for a firm basis for how the corridor can be evaluated and to determine its benefits.

In practice DCC will utilise its adaptive traffic control system SCATS to undertake the required traffic management on the corridor to enable the public transport corridor to perform as per the requirements. Because of the use of a real world system which has multiple inputs from the Bus AVL system, cycle and pedestrian detection as well as vehicle actuated sensors, the signals will be running multiple sets of timings across the day rather than a fixed set of timings and the use of this technology will allow improved corridor operation.

The design of this scheme in the Dublin City Council area is difficult and complex and has called for multiple interventions along the road network in order to achieve its objectives. The use of bus priority signals, turn bans, bus gates and other interventions are all intended to alter the current traffic situation along the route and ensure that Public Transport walking and cycling can be prioritised over the private car.

It should be noted that this corridor needs to be considered as a whole and that the various different measures to prioritise public transport walking and cycling, need to be implemented in as full a manner as possible to avoid “watering down “ the benefits of this scheme by making localised changes to the design.

Because of the nature of the turn bans, bus gates and the use of signal controlled priority the deployment of Camera based bus lane enforcement will need to have been rolled out on this corridor before the full benefit of the scheme in terms of bus journey reliability can be achieved.

The enhanced data garnered by DCC from the next Generation AVL system and the next generation Bus priority system currently being specified will play a key role in how the corridor is dynamically managed to ensure that the bus journey times and headways are met.

This digital infrastructure along with the proposed civil infrastructure for traffic signals are both required for the corridor to meet its objectives



### Project Delivery Mechanism

This project is being undertaken by the NTA in the role of public transport regulator exercising the right to provide improvements to public transport infrastructure directly via Section 51 of the 1993 Roads Act. The NTA is taking over the role of the Road Authority for the purposes of obtaining planning permission for the corridors and the subsequent construction of the corridors will be undertaken directly by the NTA via their contractors.

Thus the planning and construction of these corridors takes more the form of the Light Rail process than for example the early Quality Bus Corridors, which were all developed and put in place directly by DCC.

### Roads Department Comments

This section of the Environment & Transportation report on the Kimmage to City Centre Bus Connects Scheme has been prepared by the Roads Department. It includes technical input from Roads Design, Roads Construction, Roads Maintenance and Transportation Planning Sections, the remit of which covers design and construction phases through to maintenance and operational phases of the scheme as well as wider policy and planning considerations. The Roads Department is generally supportive of the scheme and its intention to improve bus and cycling provision. Having reviewed the application documentation, the department would like to highlight some matters which, with further consideration, could improve the scheme. The comments set out in the first instance are generally applicable to all the schemes. The Roads Department has in response to these matters developed a set of recommended standard conditions for attachment to all permissions granted which, once complied with, will facilitate engagement and agreement between DCC and the NTA at detailed design and construction stages. Scheme specific comments are also highlighted below for An Bord Pleanála's consideration.

In general terms, Bus Connects proposes substantial improvements to bus and cycling infrastructure, with provision of additional signalised crossings for pedestrians along the routes. The schemes, including the Kimmage scheme, could be improved by making greater provision for pedestrians by ensuring sufficient and appropriate footpath widths based on pedestrian flows (with an absolute minimum 2m width) and also by ensuring pedestrian priority throughout the routes. There are recurring situations throughout the schemes where user priority is unclear, for example at bus stops and where cycle routes cross footpaths. Grade or physical separation between cycling facilities and footpaths is recommended and running cycle tracks through footpaths and pedestrianised zones should be avoided. Ensuring pedestrian priority is important particularly in the context of people with accessibility issues including visual impairments. Pedestrians, in accordance with all levels of policy, should be ensured priority through signage and other appropriate measures. A condition is recommended in this regard.

Another design feature of all schemes is the reallocation of kerbside space to buses and cyclists, the impact of which is the removal of potential kerbside loading and servicing. This activity is crucial for the general functional operation of the city and to the achievement of the 15 Minute City where people can walk and cycle to local shops and services. Safeguarding the ability of local services to operate is therefore imperative. The extent of loss of loading bays is not clearly quantified in the schemes, nor is the adequacy of alternative provision demonstrated. More information and clarity in

this regard would provide comfort that the scheme will continue to support the operation of local businesses. In addition to loading facilities, on street parking is also affected including at commercial units. A condition regarding loading and parking is attached.

Regarding the current scheme, the following location specific comments are provided on a sheet number basis.

#### Location Specific Comments

##### *Sheet 1*

The location of proposed trees on the west side of Kimmage Road Lower needs to be carefully considered so as to minimise obstruction of footpath. A minimum of 2m clear unobstructed width is required.

##### *Sheet 2*

No location specific comments.

##### *Sheet 3*

The location of proposed trees needs to be carefully considered so as not to obstruct footpath. A minimum of 2m clear unobstructed width is required. Proposed trees should also not impede sightlines. Car parking appears to be proposed too close to the Sundrive Road junction on both sides. This should be further set back from the junction as well as from pedestrian crossings.

Section E-E Kimmage Road Lower shows parking as 2.3m width with 3m lane widths. This is narrower than the desirable width of 2.4m (DMURS).

##### *Sheet 4*

At the revised junction layout of Mount Argus View and Kimmage Road Lower tree planting should take cognisance of sightlines and ensure unobstructed footpaths. At the revised Mount Argus Church junction the location of trees and parking should not be too close to junction. Parallel parking is preferred to perpendicular for safety reasons.

##### *Sheet 5*

No location specific comments.

##### *Sheet 6*

It is noted that a loading bay is proposed inside the bus gate. Signage should possibly clarify that loading is also permissible as well as local access.

The submitted drawing does not appear to take cognisance of the St. Clare's Park development including its access junction layout. There is a current proposal to relocate the existing bus stop southwards away from the St. Clare's junction. The new position has been agreed between the developer, Dublin City Council and the National Transport Authority.

There are serious concerns regarding the removal of the footpath along the southern boundary of Harold's Cross Park and the absence of crossing facilities for pedestrians to link to the opposite

footpath. It is proposed that the footpath ends abruptly. However pedestrians are not directed to a crossing point which would allow them to safely access the southern opposite footpath.

Proposed trees within footpaths should be sited so as to ensure minimum 2m unobstructed footpath.

#### *Sheet 7*

Proposed access arrangements to the new car park within Our Lady's Hospice grounds are not clear. It is also not clear what is proposed regarding the main access arrangements to the Our Lady's Hospice campus from Harolds Cross Road. It would appear that the footpath is being extended across the junction with a cycle lane adjacent therefore it is not clear how vehicular access is being provided at this location. The submitted documentation indicates that this car park will be available for residents along Harolds Cross Road. However, it is not clear how this could be managed having regard to the fact that the car park is to be located within a private site and be accessed from a private road. For Dublin City Council to manage the car park including public access to same, it and the access road would have to be taken in charge by Dublin City Council.

At the revised Mount Drummond Junction an alternative car parking arrangement should be considered in the redesign. Parallel parking is preferred to perpendicular parking for safety reasons. Parking should also be located further away from the junction.

#### *Sheet 8*

No location specific comments.

#### *Sheet 9*

Heritage features are shown in the middle of cycle lanes/parking spaces/footpaths.

#### *Sheet 10*

Heritage features are shown in the middle of cycle lanes/parking spaces/footpaths.

#### *Sheet 11*

No location specific comments.

#### *Sheet 12*

No location specific comments.

#### *Sheet 13*

No location specific comments.

#### *Sheet 14*

No location specific comments.

#### Environmental Protection Division

Dublin City Development Plan 2022-2028, Chapter 9 identifies the need for Sustainable Environmental Infrastructure as part of any development in the city. The criteria listed in Chapter 9

are linked to the other major environmental themes within the Plan specifically in relation to Climate Change, Green Infrastructure, Open Space and Recreation, and Sustainable communities. The principles of Sustainable Drainage Systems (SuDS) should be integrated with all other environmental aspects of a project, using best practice solutions. DCC requires this softer engineered approach to be used to manage surface water at source as it is a greener, more environmentally effective approach for managing stormwater.

The key requirements for this development from a surface water/drainage/flood management perspective are outlined as follows:

This development must comply with the Greater Dublin Regional Code of Practice for Drainage Works Version 6.0 (available from [www.dublincity.ie](http://www.dublincity.ie) Forms and Downloads). In particular:

- Continuous kerbs incorporating drainage, as outlined in Figure 2, Page 3 in Appendix K Drainage Design Basis Document, are not accepted by DCC Drainage Planning, Policy and Development.
- Enclosed drainage channels such as slot drains or “ACO” drains are not accepted by Drainage Planning, Policy and Development.
- The hybrid gully outlined in Section 1.1.3, Page 4 in the BusConnects - Road run-off collection gullies Technical Paper is not accepted by DCC Drainage Planning, Policy and Development. The use of narrow profile gullies as previously agreed is welcome.

The development shall incorporate Sustainable Drainage Systems in the management of surface water, providing an integrated approach with the landscaping proposals. Full details of these shall be agreed in writing with DCC Drainage Planning, Policy and Development Control prior to commencement of construction. Soft landscaping should be considered before hard landscaping. The SuDS design should refer to the new Dublin City Council Sustainable Drainage Design and Evaluation Guide published in 2021.

The detailed drainage design shall be agreed in writing with DCC Drainage Planning, Policy and Development prior to commencement. It shall be in accordance with the requirements set out in the Greater Dublin Regional Code of Practice for Drainage Works. Surveys on the location and condition of surface water infrastructure sewers, both pre and post development, shall be carried out by the developer and any damage rectified. Any diversions shall be agreed in writing, prior to commencement, with Drainage Planning, Policy and Development Control. Details on proposed connection locations to the surface water network and flow discharges shall also be agreed.

The NTA shall confirm in writing to Drainage Planning, Policy and Development Control that the development has been designed such that the risk of flooding to the development has been reduced as far as is reasonably practicable, and that the proposals do not increase the risk of flooding to any adjacent or nearby area. The effect of climate change on flooding, +20% rainfall and 0.5m sea level rise should be allowed for in calculations.

Any changes in ground profile shall be modelled to demonstrate no increase in flood risk and to reduce it where reasonably possible.

Pluvial flood risk shall be assessed at all locations along the route. It should not be increased anywhere and should be reduced where possible. No pluvial flooding for 30 year flood scenario is welcome but needs to be connected to new SuDS/GI features rather than our already overloaded network.

The NTA must demonstrate that this development passes the three stages of the SFRA Justification Test, particularly for fluvial flooding.

New compensatory SuDS measures should be close to any green areas lost.

Flow control manholes to be clearly identified throughout the design as it would allow a better understanding of the design and how the attenuation is proposed to work.

**The following more detailed comments shall be addressed:**

1. Ch A0-A190, A340-410, A440-A520, The introduction of trees over existing piped networks should be assessed as this may cause issues in the future with root growth etc.
2. Ch H70070-70120, How is this additional impermeable area attenuated, no detail provided? Does the runoff from this impermeable area flow directly into the Poddle untreated?
3. Ch 70340, Areas suitable for use as bio retention areas should be utilised where possible.
4. Ch B10448, why is this permeable paving area not connecting to the new piped network? The outfall is not connected to any existing network?
5. Ch A2345-2660, Design to include catchment description, detail on the volumes required / provided and proposed flow rates. The location of the proposed Hydrobrake should be included to allow an understanding of the design.
6. Ch A2700, How is this compensatory attenuation provided and how much? More detail required in the design.
7. Drawing BCIDD-ROT-DNG\_RD-0011\_XX\_00-DR-CD-1001 and BCIDD-ROT-DNG\_RD-0011\_XX\_00-DR-CD-1002 do not give enough detail regarding the discharge points for each catchment.

**Water Framework Directive**

The proposed Kimmage to City Centre Core Bus Corridor Scheme transverses the catchment of the River Poddle within the Dublin City Council administrative area. Albeit all waterbodies are subject to the European Union Water Framework Directive, this waterbody is included in the River Basin Management Plan (RBMP) as a 'second tier river' with a requirement to protect and restore the river status to a 'good' designation or better, in addition to being protected under Article 4 of the WFD. Currently the Poddle is of 'poor' status.

Dublin City Council is obliged to achieve a water quality status of 'good' or better with all waterbodies by December 2027. To support our achievement of our legislative obligations, the proposal should not cause a deterioration of the status of any waterbody which it is contiguous with downstream and furthermore should not jeopardise the attainment of good ecological and good surface water chemical status, in accordance with our obligations. In particular, all surface water that discharges from the curtilages of the Kimmage to City Centre CBC Scheme proposal into existing or proposed waterbodies should be intercepted and treated, using nature based solutions wherever possible.

Where possible, drainage within the curtilage of this project should be segregated, and infrastructure for discharging surface water into existing surface water sewers should be implemented.

Good Status includes both good ecological and chemical status as determined by the Environmental Protection Agency against an established set of assessment criteria. The latest status indicators may be viewed at [www.catchments.ie](http://www.catchments.ie).

In the Environmental Impact Assessment Report, Chapter 13, we welcome the acknowledgement that urban runoff is a significant pressure on the receiving waters within the project area. However, we do not agree or accept the report's argument in regard to the 'Sensitivity of Receptors', section 13.2.4.2. The report includes an extract from the National Roads Authority, which seems to indicate that the lower the status of a water body, the less sensitive the receptor is. This insinuates that a water body, which has not yet achieved the legislative requirements set out in the EU Water Framework Directive ('Good' ecological status), may receive surface water run-off of a lower quality than 'Good'. We maintain the EU Water Framework Directive takes priority and overrides the National Road Authority and the UK Environment Agency as referenced within section 13.2.4.2 of the report.

As a Member State, Ireland is required to improve the status of ALL water bodies. Chapter 13 goes on to state that the implementation of the 3rd Cycle River Basin Management Plan should address the pressures on the receiving waters, including urban runoff. However, given the scale of the proposed project and our legal requirement to meet the EU WFD obligations by 2027, the project needs to support and be consistent with the delivery of that 3rd Cycle RBMP. While the local authority is responsible for overseeing the implementation of programmes of measures, all stakeholders need to be involved in delivering the RBMP, including the proposed project. Urban runoff is a significant urban pressure, and the Bus Connects schemes are the single biggest planned intervention to key, heavily trafficked, commuter routes into the city.

It should be noted that while the Ringsend Wastewater Treatment Plant is listed as the receptor for Surface Water runoff draining into the combined sewer this, this runoff can still affect adjacent waterbodies via Combined Sewer Overflows during rainfall events.

The developer shall provide an evidence-based assessment of the impact, if any, of the proposed scheme on the water quality status of the rivers within the curtilage of the proposed project, including both ecological and chemical status.

#### **Flood Prevention**

At detailed design stage more detail will need to be provided and agreed on:

- Plan for dealing with local pluvial flooded areas anywhere where flood depths on the carriageway are predicted to be above 300mm.

#### **4.12.2 Recommended Conditions**

See Appendix.

## **APPENDIX 1 RECOMMENDATIONS / CONDITIONS**

### **Conditions to be agreed between Dublin City Council and the National Transport Authority**

1. That a comprehensive agreement is put in place between DCC and the NTA regarding how the corridor is to be handed over to the NTA and its contractors, what pre-inspection and recording of the corridor is necessary and how the corridor is to be maintained during construction activities and by whom. The agreement shall also address the handback process, the treatment of all relevant records treated and how the corridor is to be accepted back by DCC following construction.
2. Following handback, a separate agreement shall be put in place between DCC and the NTA regarding the costs of maintenance of the corridor as a high quality public transport corridor with agreed levels of performance and how the performance of the public transport corridor is not eroded in the future.
3. All relevant DCC departments involved with the development of the Scheme shall be consulted during the detailed design development process for the Scheme and the NTA shall incorporate the requirements of the DCC departments into the final detailed design of the Scheme.

### **Suggested Conditions**

#### **City Archaeologist**

1. NTA to appoint a Project Archaeologist as a member of the NTA project team to oversee all archaeological aspects of the project from inception to completion. The Project Archaeologist will manage archaeological aspects of the project and input on, inter alia:
  - a. Project planning and design,
  - b. Scheduling of archaeological mitigation,
  - c. The development of programmes,
  - d. The development of construction and procurement strategies,
  - e. The preparation of contract documentation,
  - f. The appointment of competent consultant archaeologists,
  - g. Advance works, construction and potential operational issues.
2. The Project Archaeologist shall ensure that the process of identifying the potential impact of the project on archaeology is dealt with by a competent archaeologist.
3. The Project Archaeologist shall oversee the archaeological operations carried out by the contractor's archaeological consultant.
4. The Project Archaeologist shall ensure that appropriate investigation is carried out, where reasonably practicable, prior to the commencement of construction to identify both the known and unknown archaeology that may be impacted by the project. Where this is not reasonably practicable, an appropriate archaeological strategy to mitigate the known or potential archaeological impacts to be developed in consultation with the Minister.
5. The Project Archaeologist shall consider whether the archaeology can be preserved in situ within the confines of the project. Where preservation in situ cannot reasonably be achieved, allow

sufficient time to preserve by record all archaeological remains that are impacted by the project to a level that is acceptable to the Minister.

6. The NTA shall provide the necessary funding to fulfil the post-excavation and reporting requirement(s) of the project to a standard that is acceptable to the Minister.
7. The Project Archaeologist shall ensure the publication and/or dissemination, as appropriate, the archaeological results of the project.
8. The Project Archaeologist shall copy Dublin City Council Archaeology Section with all Section 26 method statements and any reports arising and provide regular updates on finds and mitigation throughout the delivery of the scheme through to completion.
9. The primary archaeological paper archive for all archaeological site investigations to be prepared and deposited with the Dublin City Archaeological Archives within a timeframe to be agreed with the planning authority unless otherwise agreed with the Minister.
10. The NTA shall provide revised proposals including detailed designs for the Poddle Boardwalk and information signage which shall enhance both the setting and interpretation of the monument, to be submitted for the written agreement of the Planning Authority prior to the commencement of development.
11. The NTA shall provide revised proposals for works at the Robert Emmett Bridge addressing potential injury to the setting of the bridge, the loss of historic walling, and safe access to the Robert Emmett Memorial, to be submitted for the written agreement of the Planning Authority prior to the commencement of development.

#### **Parks, Biodiversity and Landscape Services**

1. Tree planting species should be planted at a minimum of 16-18cm girth with a minimum of 3 years post practical completion maintenance to ensure healthy establishment. Much of the proposed tree planting will need to be installed in constructed tree pits with 15 cu.m of growing medium, details of these pits should be agreed with this department.
2. Tree Bond  
Prior to commencement of development, the applicant shall lodge with the planning authority a cash deposit , a bond of an insurance company or such other security as may be accepted by the planning authority to secure the protection of existing trees to be retained on or adjacent the site and to make good any damage caused by construction, coupled with an agreement empowering the planning authority to apply such security , or part thereof, to the satisfactory protection of any trees adjacent or on the site or the replacement of any such trees which die, are removed or become seriously damaged or diseased within a period of three years from the substantial completion of the development with others of similar size and species, or to apply to new tree planting in the local area. The amount of the security shall be determined by the Helliwell or Cavat method by the developer's arboriculturist. The form and amount of the security shall be as agreed between the planning authority and the developer or, in default of an agreement, shall be referred to An Bord Pleanála for determination.  
Reason: To secure the retention of existing trees to be retained on the site.
3. Landscape scheme to be implemented



The applicant will retain the professional services of a qualified Landscape Architect as a Landscape Consultant throughout the life of the development works. The landscape scheme accompanying the application as amended with comments given by the planning authority shall be implemented fully in the first planting season following completion of the development or completion of any phase of the development, and any vegetation which dies or is removed within 3 years of planting shall be replaced in the first planting season thereafter. The implemented landscape scheme will be maintained for 3 years post completion by the applicant. The applicant's landscape architect will submit a Landscape Completion Report to the planning authority for written agreement, as verification that the approved landscape plans and specification have been fully implemented.

The landscape scheme shall have regard to the Guidelines for Open Space Development and Taking in Charge, copies of which are available from the Parks and Landscape Services Division.  
Reason: in the interests of amenity, ecology and sustainable development

4. Tree Protection

Prior to the commencement of development, the developer will retain the professional services of a qualified Arboriculturist throughout the life of the site development works. The Arboriculturist will advise and supervise all works associated or in proximity to the existing trees to ensure their retention and condition. All trees shown to be retained on the site and adjacent to the site, shall be adequately protected during the period of construction as per BS 5837 and the Arboricultural Impact Assessment Report such measures to include a protection fence beyond the branch spread, with no construction work or storage carried out within the protective barrier. A tree protection plan shall be submitted to Dublin City Council for written approval prior to the commencement of works. The arboriculturist will prepare a tree survey on completion of the works and submit to the planning authority. All outstanding remedial tree works will be completed by the applicant as directed by the planning authority. (The tree protection measures shall have regard to the Guidelines for Open Space Development and Taking in Charge, copies of which are available from the Parks and Landscape Services Division).

Reason: in the interests of amenity, ecology and sustainable development.

5. Bat & Bird Conservation

(a) All trees proposed to be felled shall be checked pre-construction for the presence of bats and a derogation licence shall be sought from the NPWS should evidence of bat activity be found. No works are to take place prior to the granting of such licences.

(b) All tree/shrub removal works shall take place outside of the bird nesting season (March 1st to August 31<sup>st</sup>.)

Reason: in the interests of protection of biodiversity.

6. Natura Impact Statement

All mitigation measures within the Natura Impact Assessment shall be implemented under the guidance and supervision of a qualified ecologist appointed by the applicant.

Reason: in the interests of ecology and sustainable development.

**City Architects Department**

1. Confirmation is requested that pedestrian traffic counts have been undertaken to ensure that the proposed footpath widths along the Proposed Scheme are sufficient to cater for anticipated pedestrian volumes. This confirmation should be submitted to the planning authority prior to commencement of development.

2. Detailed drawings and specifications of the proposed public realm improvement schemes shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
3. Details of all landscaping and public realm finishes in areas where they are to be taken in charge shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
4. Full details of the design and type of each bus shelter for each location shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
5. The siting of all utility cabinets and other above-ground utility infrastructure shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
6. The NTA should engage with electrical charging operators to co-ordinate the roll out of electrical charging points to on-street parking areas as part of the works along the route of the Proposed Scheme. This shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
7. The extent of existing hard landscape to be retained within the Proposed Scheme shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
8. The material palette within the Proposed Scheme shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
9. A full palette of street furniture and their proposed locations across all the proposed BusConnects Core Bus Corridor Schemes, shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
10. The fabric in all property boundaries which are to be relocated to facilitate land acquisition along the Proposed Scheme should be assessed for their architectural conservation value and cultural value. This assessment should be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
11. The fabric in all property boundaries which are to be relocated to facilitate land acquisition along the Proposed Scheme should be assessed whether it may be suitable for repair and re-use for sustainability reasons in the new boundaries rather than replaced with new. This assessment should be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
12. Revised proposals for the Emmet Bridge addressing the concerns raised by the City Architects Division are to be submitted and agreed with DCC Planning and Conservation section prior to commencement of development.

13. Further information is required on the design of the Stoneboat Bridge/ boardwalk, measures to address any potential anti-social behaviour and a full landscaping proposal are to be submitted and agreed with DCC Planning department prior to commencement of development.
14. The selection and location of artworks along the route as part of the Percent for Art strategy shall be reviewed and agreed with the local authority Arts Office and submitted to, and agreed in writing with, the planning authority prior to commencement of development.
15. All painted medians should be reviewed to ascertain if they could provide additional greenery or alternatively if the median space could be removed and reallocated into the footpaths adjacent. This shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
16. The number of poles required for traffic signal and signage needs to be designed to the minimum. This information shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
17. Public Lighting Division, DCC, are to be consulted on the locations and styles of public lamp standards in the city centre. This information shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.
18. Suitable locations for water drinking fountains should be identified and installed as part of the works along the route of the Proposed Scheme. This shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development
19. Village signage should be incorporated into the Proposed Scheme, and a scheme of city wide co-ordinated village signage should be undertaken and shall be submitted to, and agreed in writing with, the planning authority prior to commencement of development.

#### **Air Quality Monitoring and Noise Control Unit**

1. A Construction Management Plan shall be submitted to, and agreed in writing, by the Planning Authority, prior to commencement of development. This plan shall be developed with reference to the 'Construction and Demolition Good Practice Guide' produced by the Air Quality Monitoring and Noise Control Unit of Dublin City Council (access link).  
<https://www.dublincity.ie/residential/environment/air-quality-monitoring-and-noise-control-unit/good-practice-guide-construction-and-demolition>
2. The hours of operation during the construction phase shall be restricted to 7.00am to 6pm, Monday to Friday, and 8.00am to 2.00pm on Saturdays. Permission to work outside of these hours must be subject to the approval of Dublin City Council.

#### **Conservation Section**

1. To safeguard the special architectural interest of affected Architectural Heritage across the Bus Connects routes - including Protected Structures and Conservation Areas, landscaping, historic paving, setts, kerbing and associated features, boundary treatments, historic street furniture,

gardens and trees and historic public realm etc. - and to ensure that the proposed works will be carried out in accordance with best conservation practice with no unauthorised or unnecessary damage or loss of historic fabric, the Conservation Section recommend that all works shall be designed and supervised by an expert in architectural conservation in accordance with the provisions (outlined above) of the Dublin City Development Plan 2022-2028, the Architectural Heritage Protection Guidelines for Planning Authorities (2011) and relevant documents of the DHLGH Advice Series.

2. The conservation professional shall ensure adequate protection of the retained and historic fabric during the proposed works and across all preparatory and construction phases. In this regard, all works shall be designed to cause minimum interference to historic fabric.
3. In accordance with best conservation practice, specifications and method statements for the careful and sensitive relocation and reinstatement of historic fabric identified in the report above, and in particular to Protected Structures, sites/structures on the NIAH and DCIHR, and structures and features in Architectural Conservation Areas (ACAs) across the Bus Connects route shall be submitted by the conservation professional for the written approval of the Planning Authority in advance of works commencing.
4. The conservation professional shall advise the Conservation Section on architectural heritage and conservation matters that may have further impacts on the project throughout the construction phases.
5. If, through the course of construction work across the Bus Connects routes, hitherto unknown and concealed architectural heritage fabric is found, the conservation professional shall contact the Conservation Section to advise them of the discovery as the presence of historic fabric may inform an alternative strategy for a design proposal that would enhance the setting of a Protected Structure, other historic buildings and features, an Architectural Conservation Area or Conservation Area.
6. All works shall be carried out in accordance with best conservation practice, the Architectural Heritage Protection Guidelines for Planning Authorities (2011) and the Advice Series issued by the Department of the Housing, Local Government and Heritage. All repair works shall retain the maximum amount of surviving historic fabric in situ. Items to be removed for repair off-site shall be recorded prior to removal, catalogued and numbered to allow for authentic re-instatement.
7. All existing original architectural heritage features, in the vicinity of the works shall be protected during the course of all phases of construction works.
8. All repair of historic fabric shall be scheduled and carried out by appropriately experienced conservators of historic fabric.
9. The Conservation Section recommends the following specific measures:
  - a. A redesigned scheme at Robert Emmet Bridge that is of higher architectural quality than the submitted proposal and that would lessen the physical and visual impact on the

historic masonry bridge shall be submitted for the written approval of the Planning Authority in advance of works commencing. New work and materials should be of the highest possible architectural quality, should complement the historic features of the bridge and should be legible as new interventions. This redesign shall be informed by a revised architectural heritage impact assessment, by a suitably qualified conservation professional, providing detailed design information and outlining the nature and likely impacts of the proposed bridge extension.

- b. The concealment / burial of historic walls at Clanbrassil Street Upper is not appropriate. The Conservation Section recommends that in lieu of burial, the maximum amount of surviving historic fabric is carefully lifted and reinstated in its new location in accordance with best conservation principles. The historic fabric shall be recorded prior to removal, catalogued and numbered to allow for authentic re-instatement. Where new construction is proposed, all new work and materials should be of the highest possible architectural quality and should complement the historic features of the wall.
- c. A redesign of the scheme at Clanbrassil Street Upper to lessen the physical and visual impact on the historic masonry walls shall be submitted for the written approval of the Planning Authority in advance of works commencing. The redesign shall be informed by a revised architectural heritage impact assessment, by a suitably qualified conservation professional, providing detailed design information and outlining the nature and likely impacts of the proposed demolition and subsequent replacement of the walls.
- d. Full details of the design and type and location of each bus shelter / stop along the proposed route in front of Protected Structures and structures on the NIAH shall be submitted to and agreed in writing with the Planning Authority in advance of works commencing.
- e. The Conservation Section recommends the omission of bus shelters in front of and in the immediate vicinity of Protected Structures across the route and for bus stops only to be considered at these locations, in order to minimise visual clutter and protect the special architectural character of Protected Structures. This shall be confirmed in writing to the Planning Authority in advance of works commencing.
- f. Consideration should be given to the rationalisation of all traffic infrastructure such as signage, traffic poles, utility boxes etc. across the route to reduce visual clutter, in particular in the vicinity of Protected Structures, within Architectural Conservation Areas, red-hatched conservation areas and in residential conservation areas.
- g. The Conservation Section recommends the omission of cantilevered signal poles in the vicinity of Protected Structures, within Conservation Areas, red hatched conservation areas and residential conservation areas and alternative traffic signalling solutions should be sought
- h. Consideration should be given to the omission of gantry traffic signage in the vicinity of Protected Structures, within Architectural Conservation Areas, red hatched conservation areas and residential conservation areas and alternative traffic signage solutions should be sought.
- i. Where cycle ways are located in close proximity to Protected Structures and within Conservation Areas generally, the Conservation Section recommends the use of alternative high quality cycle lane surface in-lieu of red tarmacadam.

- j. The alignment of footpaths should respect the setting of Protected Structures and buildings of National importance.

## **Environmental & Transportation Department**

### **Traffic Division**

1. All the traffic management equipment that is necessary for the safe and efficient operation of this Public Transport corridor, including all traffic signal equipment, shall be to the relevant DCC specification and only the relevant DCC maintenance contractor shall be permitted to undertake electrical or system control work on either the existing or new traffic signals.

### **Roads Division**

#### Handover

1. Prior to commencement of any works, a formal Handover Procedure Agreement shall be agreed with Dublin City Council and put in place. This procedure shall be carried out on any section of work as soon as it is completed. A global handover of all works at the end of the construction period shall not be permitted. As built drawings of each section of the finished works shall be provided in A1 sized hard copy to an appropriate scale and also in electronic format compatible with DCC's current version of Microstation. These as built drawings shall include details of new services and alterations to existing services. Drawings shall also be provided showing exactly what areas are to be in DCC's charge.

#### Existing Condition Record

2. A photographic record of all areas in Dublin City Council's control to be affected by the Bus connects scheme works shall be provided to Dublin City Council (DCC) prior to the commencement of any work.
3. Drawings distinguishing between antique granite footways and kerbs and new granite footways and kerbs shall be submitted as part of detailed design development of approved scheme.
4. Drawings clearly demarcating private landings shall submitted as part of detailed design development of approved scheme.

#### Design

5. Final details (including materials, finishes, sizes, gradients, levels and drainage) of all junctions, carriageways, islands, buildouts and footways as well as all signal/traffic light infrastructure shall be agreed with DCC prior to construction.
6. All Construction works shall comply with the "Construction Standards for Roads and Street Works in Dublin City Council".
7. Road Safety Audits shall be carried out for each public road that is to be modified as part of the Bus Connects scheme works at appropriate stages throughout the design of each individual scheme.
8. The alignment of the Bus Connects scheme shall be designed so as ensure that all longitudinal gradients and crossfalls on carriageways, islands, buildouts and footways are in

accordance with those specified in "Construction Standards for Road and Street Works in Dublin City Council" unless otherwise agreed with DCC.

9. Pedestrian priority shall be ensured throughout the Scheme design through signage and physical design measures where appropriate.
10. Buffer strips shall be provided at all locations where cycle lanes run between parking and loading areas and the kerb/footpath to ensure pedestrians including those with disabilities can safely alight from vehicles.
11. The Scheme shall ensure that principles of universal design are adhered to and accessibility requirements are met throughout the Scheme.
12. Modifications to existing in-curtilage car parking of properties impacted by the works shall ensure a footprint of 5 metres by 3 metres for car parking is retained in order to avoid parked cars overhanging the public footpath.
13. Alterations to kerbside spaces such as pay and display scheme/loading/line markings/signage pole shall be agreed with the Planning Authority to ensure adequate loading and set down is provided.
14. All signage and road markings to comply with the *Traffic Signs Manual*.

#### Reinstatement

15. All reinstatement work and areas to be taken in charge shall be carried out in accordance with "Construction Standards for Road and Street Works in Dublin City Council" unless otherwise agreed with DCC.
16. The extent and type of the reinstatement required shall be agreed with DCC prior to commencement of any work on site. This shall be shown on drawings and signed off on by both parties.
17. All works to public roads in DCC's Functional Area shall comply with the Council's *Construction Standards for Road and Street Works in Dublin City*.
18. Samples of all new natural stone kerbs, flags and setts to be used in reinstatement works shall be supplied to DCC for agreement prior to use.

#### Construction Period

19. All roadworks shall be carried out in accordance with the current edition of Dublin City Council's *Directive for the Control and Management of Roadworks in Dublin City* unless otherwise agreed with DCC.
20. In cases of reinstatement of areas where the roadway or footway is not being reconstructed in full (e.g. trench for utility alongside street) the NTA or their Contractor shall pay DCC long term damages charges as set out in the current edition of Dublin City Council's *Directive for the Control and Management of Roadworks in Dublin City*.



21. All antique setts if removed as part of the works shall be cleaned, stored on pallets by the contractor and reinstated in the carriageway to DCC's specification if required by DCC unless otherwise agreed with Dublin City Council.
22. All existing and antique natural stone kerbs and flags, if removed without damage as part of the works, shall be cleaned, stored on pallets by the contractor and reinstated in the footway to DCC's specification.
23. During construction and prior to opening of the Scheme, the National Transport Authority shall undertake an awareness, education and behavioural change programme to educate road users as how to use the Scheme with particular regard to interaction between pedestrians and cyclists.

#### Miscellaneous

24. Where cellars exist and are effected by the scheme, these shall be acquired in whole or in part only where necessary for implementation of the proposed scheme.

#### **Public Lighting**

In terms of delivering the Public Lighting elements of this project, it is recommended that careful consideration be given during the detailed design process to all the various different elements including the required light level design and the relevant EN certification as well as existing heritage and high value lighting Columns. .

In addition there is the agreed condition for the survey and handover of all items along the corridor and these would include the Public lighting infrastructure and all associated items, careful consideration of existing and proposed trees within the corridor is also required as to their impact on lighting levels.

1. It must be noted that special consideration must be given to any scheme where the Public Lighting is mounted on ESB Networks Infrastructure.
2. Public Lighting works may only be carried out on street lights mounted on ESB Networks in accordance with 'ESB Requirements for Work on Public Lighting on ESB's Networks' and by Public Lighting Contractors who have the required training and approvals for such work. These requirements impose stringent requirements on Local Authorities and Contractors.
3. All heritage public lighting must be safeguarded and protected and any requirements to move heritage columns must be agreed with the Public Lighting department.
4. Temporary Lighting If the route where works are being carried out remains open for public use, e.g. to facilitate the continued movement of vehicles and pedestrians, then the route must be lighted at all times during night time hours.

#### **Environmental Protection Division**

The key requirements for this development from a surface water/drainage/flood management perspective are outlined as follows:

1. This development must comply with the Greater Dublin Regional Code of Practice for Drainage Works Version 6.0 (available from [www.dublincity.ie](http://www.dublincity.ie) Forms and Downloads). In particular:
  - o Continuous Kerbs incorporating drainage, as outlined in Figure 2, Page 3 in Appendix K Drainage Design Basis Document, are not accepted by DCC Drainage Planning, Policy and Development Control.



- Enclosed drainage channels such as slot drains or “ACO” drains are not accepted by Drainage Planning, Policy and Development Control.
  - The hybrid gully outlined in Section 1.1.3, Page 4 in the BusConnects - Road run-off collection gullies Technical Paper is not accepted by DCC Drainage Planning, Policy and Development Control. The use of narrow profile gullies as previously agreed is welcome.
2. The development shall incorporate Sustainable Drainage Systems in the management of surface water, providing an integrated approach with the landscaping proposals. Full details of these shall be agreed in writing with DCC Drainage Planning, Policy and Development Control prior to commencement of construction. Soft landscaping should be considered before hard landscaping. The SuDS design should refer to the new Dublin City Council Sustainable Drainage Design and Evaluation Guide published in 2021.
  3. The detailed drainage design shall be agreed in writing with DCC Drainage Planning, Policy and Development Control prior to commencement of construction. Surveys on the location and condition of surface water infrastructure sewers, both pre and post development, shall be carried out by the developer and any damage rectified. Any diversions shall be agreed in writing, prior to commencement, with Drainage Planning, Policy and Development Control. To avoid multiple connections to combined sewers a separate surface water network would be preferable in instances where this could be achieved. The developer shall explore all opportunities to segregate the surface water from the combined drainage system. Details on proposed connection locations to the surface water network and flow discharges shall also be agreed.
  4. To support our achievement of our legislative obligations the Kimmage to City Centre Core Bus Corridor Scheme proposal should not cause a deterioration of the status of any waterbody to which it is contiguous with downstream and furthermore should not jeopardise the attainment of good ecological and ‘good’ water chemical status for the River Poddle, in accordance with DCC and national obligations. NTA shall provide an evidence-based assessment of the impact, if any, of the proposed scheme on the water quality status of rivers within the curtilage of the proposed project, including both ecological and chemical status.
  5. The NTA shall confirm in writing to Drainage Planning, Policy and Development Control that the development has been designed such that the risk of flooding to the development has been reduced as far as is reasonably practicable, and that the proposals do not increase the risk of flooding to any adjacent or nearby area. This includes assessment of pluvial flood risk at all locations along the route. The effect of climate change on flooding, +20% rainfall and 0.5m sea level rise should be allowed for in calculations. Any changes in ground profile shall be modelled to demonstrate no increase in flood risk and to reduce it where reasonably possible.
  6. The developer must demonstrate that this development passes the three stages of the SFRA Justification Test, particularly for pluvial and fluvial flooding.
  7. New compensatory SuDS measures should be provided close to any green areas lost.
  8. As-built drawings of all drainage networks and SuDS measures shall be provided by the NTA on completion of the works.

### **Air and Noise Pollution Control Unit**

1. It is recommended that the works must be carried out having regard to a Construction Management Plan submitted with the application. The plan must be written having regard to this Unit's Good Practice Guide for Construction and Demolition (below link). The plan must be approved by the Planning Department before work commences.

<https://www.dublincity.ie/residential/environment/air-quality-monitoring-and-noise-control-unit/good-practice-guide-construction-and-demolition>



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Anthony Flynn  
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